

**AGREEMENT
BETWEEN
CLEVELAND-CUYAHOGA COUNTY PORT AUTHORITY
AND
[COMPANY TBD]
FOR THE MAINTENANCE DREDGING OF DOCKS 24E AND 26W
GENERAL CARGO TERMINAL &
PLACEMENT OF DREDGED MATERIAL
INTO THE
PORT OF CLEVELAND'S SEDIMENT PROCESSING & MANAGEMENT
FACILITY
AT
CONFINED DISPOSAL FACILITY 12
CLEVELAND HARBOR, OHIO**

THIS DREDGING CONTRACT & MATERIAL PLACEMENT AGREEMENT is entered into this ____ day of [month/ year] (“Effective Date”), by and between the [COMPANY TBD] (“Licensee” or “Dredger”) and the Cleveland-Cuyahoga County Port Authority, a body corporate and politic and political subdivision of the State of Ohio, whose street address is 1100 West 9th Street, Suite 300, Cleveland, Ohio 44113 (“Licensor” or the “Port of Cleveland”).

RECITALS:

WHEREAS, the Licensor seeks to perform maintenance dredging at the Dock 24E and 26W berths of the General Cargo Terminal where it is estimated 3,395 cubic yards of sediment (excluding over dredge allowance) requires removal under permit LRB-2007-00146 (**Exhibit C**); and,

WHEREAS, the Licensor has retained the services of an engineering consultant, KS Associates, Inc., to perform a pre-hydrographic survey to estimate the dredge volume that requires removal and to develop pre-dredge drawings as shown in **Exhibit A** and incorporated herein; and,

WHEREAS, the engineering consultant will perform a post dredge hydrographic survey within fifteen (15) calendar days of Licensee completing dredging at the berth to determine the final dredge volume pay quantity which the Owner will determine final pay quantity to the Licensee; and,

WHEREAS, the Licensor is seeking authorization from the Ohio Department of Natural Resources to perform in water work for up to five (5) calendar days during the restricted period from June 1, 2024 through June 30, 2024; and,

WHEREAS, the Licensor is the operator of a Sediment Processing & Management Facility located in Cuyahoga County, Ohio, and collectively owned by the State of Ohio, the United States Federal Government, the Port of Cleveland, and the City of Cleveland, lying generally North of North Marginal Road, adjacent to the Burke Lakefront Airport in

Cleveland Harbor, Ohio, known as CDFs 9 & 12 ("Property") containing approximately 81 acres; and,

WHEREAS, the Licensee seeks a Contractor Furnished Placement Facility where it can stockpile, store and/or manage dredged materials in connection with the dredging of the Licensor's docks at the General Cargo Terminal; and,

WHEREAS, there are portions within the Property that have been constructed and are suitable as a Contractor Furnished Placement Facility for the placement and permanent retention of the estimated 3,395 cubic yards of dredge sediment generated; and,

WHEREAS, the Ohio Environmental Protection Agency has "approved" and permitted this Site for the activities described in this Agreement.

NOW THEREFORE, the Licensor and Licensee agree: The above recitals are incorporated herein and made a part hereof.

ARTICLE I – PREMISES

The Port of Cleveland, as the Licensor, hereby grants unto the Dredger, as the Licensee, the right to use that portion of the Property/Facility (referred to as Contractor Furnished Placement Facility) consisting of the Mechanical Silt Basin, as more particularly depicted on **Exhibit "B"** attached hereto and made a part hereof ("Premises"), pursuant to all terms and conditions herein.

ARTICLE II – DURATION OF AGREEMENT & TERMINATION

This Agreement shall remain in effect for 60 calendar days following the Effective Date of execution. This Agreement may be terminated earlier or extended by the Licensor notifying the Licensee, in writing, of the Licensor's intent to terminate this Agreement.

ARTICLE III – USE OF PREMISES; GENERAL CONDITIONS

A. The Licensee may use the Sediment Processing & Management Facility for stockpiling, storing and/or final placement of the dredged materials only. No rubble (demolition debris or significant quantities of timber) or non-dredged material may be delivered unless stockpiled, loaded out, and disposed of by the Licensee. All temporary staging and material brought in to complete the work must be removed and the site restored to its original condition.

B. For the duration of this Agreement, dredged materials delivered to the CDF shall be the sole property of the Licensor absent of a written document signed by the Licensor wherein the Licensor transfers or assigns the dredged materials to a designated third party or to the Licensee as provided for in this Agreement, at the Licensor's sole option.

C. The Licensee understands that this site is active, and the Licensor may have other Contractors working on site processing dredge material, working on capital projects, or possibly performing other adjacent dredge offload operations. The Licensee shall

perform its placement operations in a manner which shall not impair, limit the progress of other Contractor's working on site, or damage the work of the other Contractors.

D. The Licensee shall not use or permit the use of the Premises for any other purpose other than that stated above and shall not grant access to any third party without the prior written permission of the Licensor and the City of Cleveland.

E. The Licensee shall comply with all applicable Federal, State, and local laws and regulations, and permits, including but not limited to the National Environmental Policy Act and Section 401 of the Federal Water Pollution Control Act (33 U.S.C. 1341). The Licensee shall implement best management practices to avoid spilling or releasing dredge material back into the Lake. All material shall be directed into the basin. Licensor reserves the right to suspend placement activities or require the Licensee to take measures if conditions are observed to be inadequate, at no additional cost to the Licensor.

F. The Licensor reserves the right to direct the Licensee to discharge materials into a given area of the site and limit placement in other areas.

G. Prior to the commencement of dredging at the Terminal and dredged placement into the Facility, the Licensor may request the Licensee submit a proposed Dredged Materials Management Plan that describes general procedures that will be implemented prior to, during, and after offloading of dredged materials including a project placement schedule, sequence of placement operations, proposed staging areas, mobilization and demobilization sequence, office and material storage location, and all other pertinent items. These plans are subject to review and approval by the Licensor and no dredged material offloading activities shall commence until written approval of the Plan is obtained from the Licensor. The Licensor may subsequently require modification or updating of the Plan if such actions are warranted, in the sole discretion of the Licensor, if deemed necessary to comply with the Site operation management practices and conditions encountered.

H. The Licensee shall notify the Licensor of any unusual odors or material variation that the dredger witnessed, any site issues the Licensor should be made aware of, and or any other relevant information the Licensor should be made aware of regarding dredging at the Terminal or placement into the Sediment Processing & Management Facility.

I. The Licensee shall place/offload up to 3,395 cubic yards of dredged material (including inherent water and exclusive of any over dredge quantity) from the General Cargo Terminal, into the mechanical silt basin as identified on Exhibit "B". The placement of dredged material shall include all means and methods, and all necessary on-site delivery conveyance and transport equipment, to offload the material from the scows and to place the dredged material within the designated mechanical silt basin at the Licensee's sole expense. Introduction of water for conveyance of this material is not permitted. The Contractor shall take efforts to minimize the sediment to water ratio and all free water shall be pumped off of the barge prior to placement of material into our Facility. The Licensor may need to import structural fill or matting to establish a working pad for their equipment. At no time shall the Licensee's equipment that is staged on top of the Licensor's provided elevated mechanical offloading platform exert concentrated loads in excess of 1,500 psf. A minimum 3' offset shall be maintained from the face of the existing wire wall. If the Licensee chooses to stage equipment in areas other than the elevated mechanical offloading pad, the Licensee shall be responsible for engineering,

berm modification/restoration, constructing, and verifying of stability and integrity of the working platforms and operating pads constructed for their equipment. The Licensee shall be responsible for restoring the site to pre-existing conditions prior to demobilizing from the site, which also includes the restoration of any established vegetation to limit erosion.

J. Licensee accessing and performing operations at the Facility will be required to comply with the requirements of the Facility's Construction Safety & Phasing Plan (CSPP), included as Exhibit D, the 7460 airspace case filed with the FAA, and shall submit a signed Safety Plan Compliance Document. As a result of site's proximity to Burke Lakefront Airport (BKL), Licensee shall closely coordinate with Burke Lakefront Airport's Assistant Operations Manager, Joel Woods at 216-664-4533. All required equipment and vehicles accessing the Facility shall be properly marked, flagged, signaled, and designated in accordance with the requirements of the Federal Aviation Administration and the site-specific Construction Safety Phasing Plan on file with the FAA. Prior to obtaining access, all employees will be required to attend Driver Safety Training course that can be scheduled through BKL, prior to obtaining access to the site. Lanyards and access credential cards will be issued by the Airport. Personal vehicles are restricted from use at the facility. Licensee shall be responsible for complying and obtaining insurance in accordance with the requirements of Burke Lakefront Airport to cross and access the CDF's. Please contact the Licensor for information related to these insurance requirements.

K. The Licensee shall not improve or otherwise place improvements on the Premises without the prior written permission of the Licensor.

L. Any needed site modifications to facilitate access to the Sediment Processing and Management Facility and the designated dredged material placement locations are to be provided by the Licensee at its sole expense. Access driveways used during operations must be maintained and restored to pre-placement conditions at the Licensee's sole expense upon completion of this Agreement. All equipment, imported materials, and debris/trash related to the placement of dredge sediment into CDF 12 shall be demobilized from the site following the completion of the work contained within this Agreement unless the dredging Contractor has other ongoing work at the Facility.

M. All persons accessing the Premises pursuant to this Agreement shall perform all work on the premises in a safe, secure, and prudent manner and in compliance with applicable Federal, State, & Local laws and regulations. The Licensee shall develop a site-specific Health & Safety Plan specific to their operations at the Facility which highlights emergency response protocol in the event of an emergency.

N. No other structures shall be constructed or maintained on the Premises except as may be approved in writing by the representative of the Licensor and may be requested to be removed prior to completion of this Agreement.

O. Prior to demobilizing from the site, Licensee shall ensure that free surface water that collects or will collect within the mechanical silt basin has positive drainage to allow water to passively drain through drainage infrastructure to the adjacent CDF 12 Settling Ponds. The drainage ditch is located at the southern end of the mechanical basins.

P. Access to the Sediment Processing & Management Facility may be prohibited or restricted during certain events, such as the week leading up to and following Labor Day or as result of distinguished guests' arrival into and out of Burke Lakefront

Airport. Licensor shall not be responsible for any damages or delays resulting from access or operational restrictions at the Facility that may be enforced by Burke Lakefront Airport, the FAA, Cleveland Airport Systems, and the City of Cleveland's Department of Port Control.

R. Licensee shall provide the Licensor a minimum of two week's advance notice prior to commencing placement operations on site in order to ensure coverage and a follow up with a 72-hour notification prior to the first delivery of dredge sediment. Licensee shall also notify the Licensor a minimum of 72 hours prior to demobilizing from site. Licensee shall not use this site to mobilize or demobilize for other Projects unless authorized by the Licensor.

S. Land based fuel tanks are not permitted at either the Terminal or the Sediment Processing or Management Facility.

T. If land access to dredge equipment is required through the Terminal, Licensee's employees shall be required to have their TWIC credentials and shall coordinate safe access with the Terminal Operator.

U. The Terminal shall remain active at all times while dredging is ongoing. Licensee shall be responsible for coordinating berth access closely with the Terminal Operator and the Licensor.

V. Licensee will be required to comply with these requirements when preparing for and while performing the offload operations as part of the contract:

1. Perform work in a safe manner in accordance with the CSPP and the 7460 on file with the FAA.
2. Existing containment berms shall not be disturbed or breached.
3. Permanent elevations within the mechanical berms shall not exceed the approved elevation on file with the FAA of +600.
4. Upon completion of the dredge cycle, cells must have positive passive drainage to the drainage ditch in the southwestern corner of the silt management cell.
5. Licensee will be required to restore any haul routes or areas outside of the silt management cell that are disturbed.
6. The Licensee is not restricted from contracting with the Licensor's Site Operator to perform support activities, so long as it does not disrupt normal site operations required to regenerate capacity for hydraulic operations.

W. In addition to this Agreement, the following additional documents are hereby incorporated by reference:

1. All provisions required by law or resolution to be inserted in the Agreement, whether actually inserted or not.
2. The Resolution of the Port Authority awarding the Agreement/Contract.
3. Amendments to the Agreement/Contract.
4. The Form Agreement & Exhibits as completed and executed.
5. All Addenda issued by the Licensor prior to Bid Submission.

6. Instruction to Bidders & Special Provisions.
7. Contract Drawings.
8. All required Policies of Insurance.
9. The Bid Affidavit of Non-Collusion.
10. U.S.A.C.E. Dredging Permit.
11. The Bid.
12. ODOT 2023 Construction & Material Specifications (C&MS).
13. Construction Safety Phasing Plan for the Sediment Processing & Management Facility on file with Federal Aviation Administration (FAA).

ARTICLE IV – LICENSEE FEE AND CUBIC YARD PAYMENT

- A. In consideration for entering into this Agreement, the Licensor agrees to pay the Licensee a fee of:
 - a. [\$XX.00] per cubic yard to dredge and offload dredged materials from the General Cargo Terminal's Docks 24E and 26W in accordance with Exhibit A.
 - b. This fee is to cover all costs related to the mob/demob, dredging, maintain active berth traffic, weather delays (if experienced), offloading, stockpiling, storing and/or final placement of the dredged materials including all costs related to hauling, moving and/or otherwise handling the dredged materials on-site (within the Property) and including all on-site maintenance, reporting, and management. This fee shall hereinafter be referred to as the "Cubic Yard Payment."
- B. The Cubic Yard Payment will be made by the Licensor to the Licensee upon final demobilization and post survey confirmation of final dredge quantity. Final quantity will be determined using the pre-dredge survey contained in Exhibit A compared against a post dredge survey performed by the Licensor performed within 15 days upon completion of the dredging at the General Cargo Terminal. No other surveys are planned by the Licensor. Licensee shall reserve their right to perform their own on site pre and post placement surveys to compare against the Licensor's final measurements. Any issues/discrepancies with the final dredge quantity shall be brought to the Licensor's within 24 hours of being sent the final post dredge and final dredge quantity.
- C. Payment will be made by delivering a check payable to the Licensee or providing an Electronic Funds Transfer of the required funds in accordance with procedures established by the Licensor and the Licensee. This payment shall be made within 30 days of the Licensor receiving an invoice from the Licensee.
- D. If the Licensor finds that the Licensee has performed operations inconsistent with any of the provisions of this Agreement or any of the referenced document to this Agreement/Contract, the Licensor reserves the right to limit or restrict the Licensee's future use of the Property in relation to other

dredging contract, add additional controls unique to the specific Licensee's use of the Property at no cost to the Licensor, and/or take actions to terminate this Agreement.

ARTICLE V – REQUIRED PERMITS

The Licensee, in its own name and at its own expense, shall obtain all applicable permits, waivers, clearances, and/or licenses required or needed in connection with the performance under this Agreement over and above those already obtained and in place by the Licensor.

ARTICLE VI – INDEMNIFICATION

The Licensee, will indemnify the Licensor and save it harmless from and against any and all claims, actions, damages, liability, and expense in connection with loss of life, personal injury and/or damage to property arising from, or out of, the occupancy or use by the Licensee or others authorized, employed or invited by the Licensee on the Premises or any part thereof or any other part of the Licensor's Property, occasioned wholly or in part by any act or omission of the Licensee, its employees, agents and/or contractors. Notwithstanding anything to the contrary in this Agreement, the parties hereto shall retain for themselves all claims and defenses under Ohio's sovereign immunity laws. The Licensor shall be required to be named as additionally insured on Licensee's policy.

ARTICLE VII - ENTIRE AGREEMENT; MODIFICATION

This Agreement constitutes the entire agreement between the parties and no oral representations, expressed or implied, shall be binding on the parties hereto. This Agreement may only be modified by written Amendment duly executed by the Licensor and the Licensee. Modification shall be required to change the terms of this Agreement in any way via Change Order.

ARTICLE VII - VENUE; ATTORNEY FEES

Any dispute, claim or action relating to or arising under this Agreement shall be brought solely in civil courts in Cuyahoga County, Ohio. This Agreement shall be governed by Ohio Law. Each party hereto agrees to bear their own attorney fees and costs in the event of any dispute, claim, action, or appeal.

IN WITNESS WHEREOF, the Licensor and the Licensee have caused this Agreement for Maintenance Dredging at Docks 24E and 26W to be executed in their respective names and their respective seals to be hereunto affixed and attested by their duly authorized officers, as of the Effective Date first written above.

LICENSEE		LICENSOR
Company Name		Cleveland-Cuyahoga County Port Authority
Name		William D. Friedman
Title		President & CEO
		Approved as to Legal Form
		Maria S. Bocanegra
		Chief Legal Officer, Cleveland-Cuyahoga County Port Authority

DRAFT

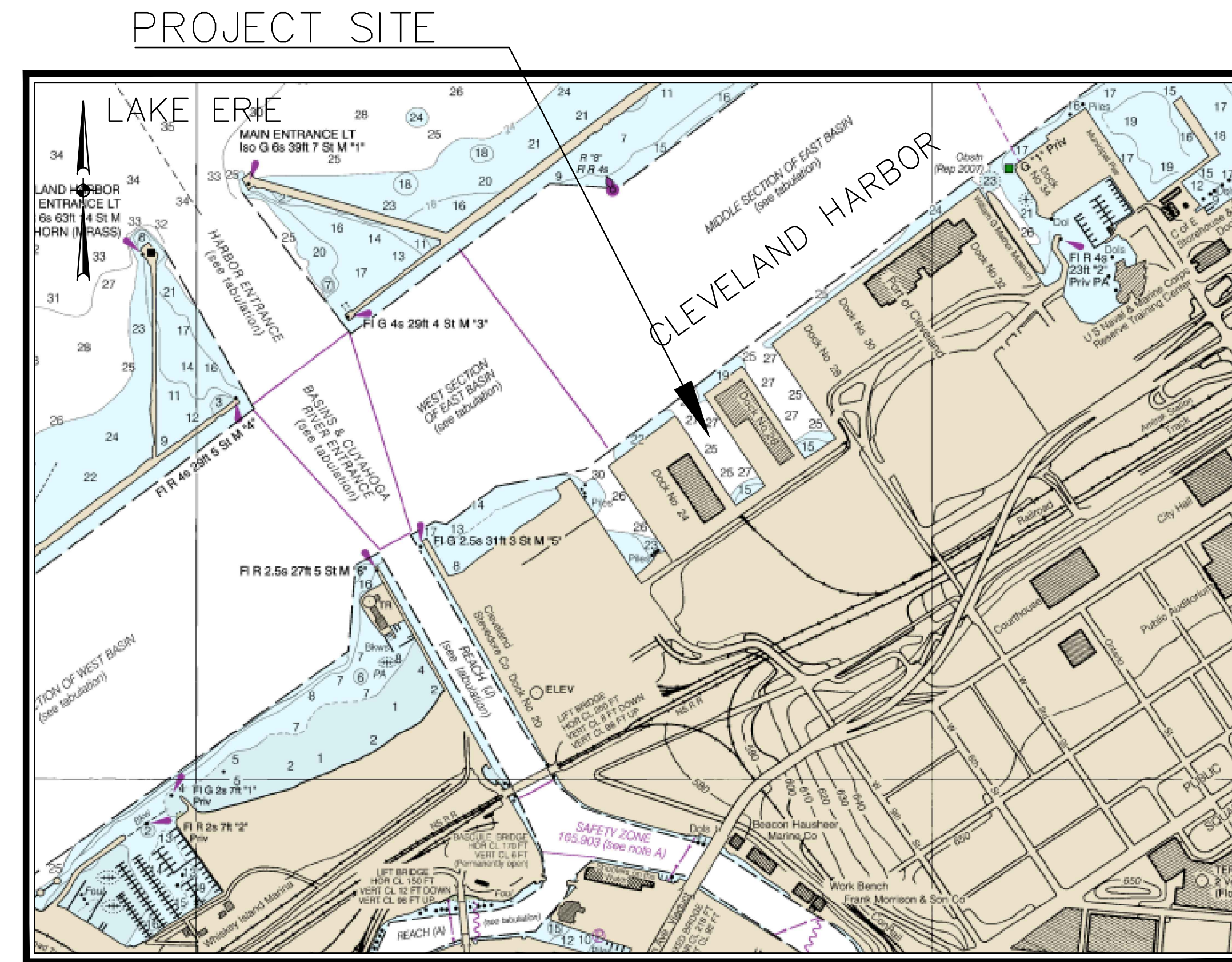
EXHIBIT A

DRAFT

CLEVELAND-CUYAHOGA PORT AUTHORITY

GENERAL CARGO TERMINAL DOCKS 24E & 26W

PRE-DREDGE SITE PLAN AND SECTIONS

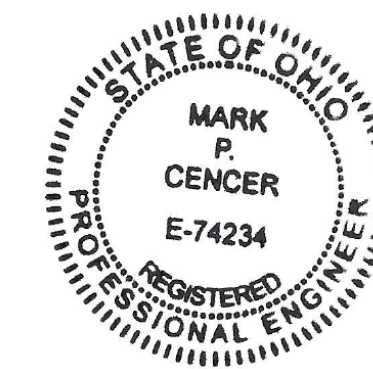


VICINITY MAP, USCG NAVIGATION CHART: 14839, NO SCALE

GENERAL NOTES:

1. CONTRACTOR SHALL FIELD VERIFY DIMENSIONS AND ELEVATIONS AS NOTED ON THE DRAWINGS. THE CONTRACTOR SHALL NOTIFY THE OWNER IMMEDIATELY OF ANY INCONSISTENCIES BETWEEN THE DRAWINGS AND THE FIELD MEASUREMENTS.

DRAWING TITLE	SHEET No.
TITLE SHEET	1
PRE-DREDGE SITE PLAN AND DREDGE AREA	2-3
SECTIONS	4-6



Mark P. Cencer

MARK P. CENCER, P.E. OHIO REG. NO. 74234

4/12/2024

DATE

DATE	DESCRIPTION	BY

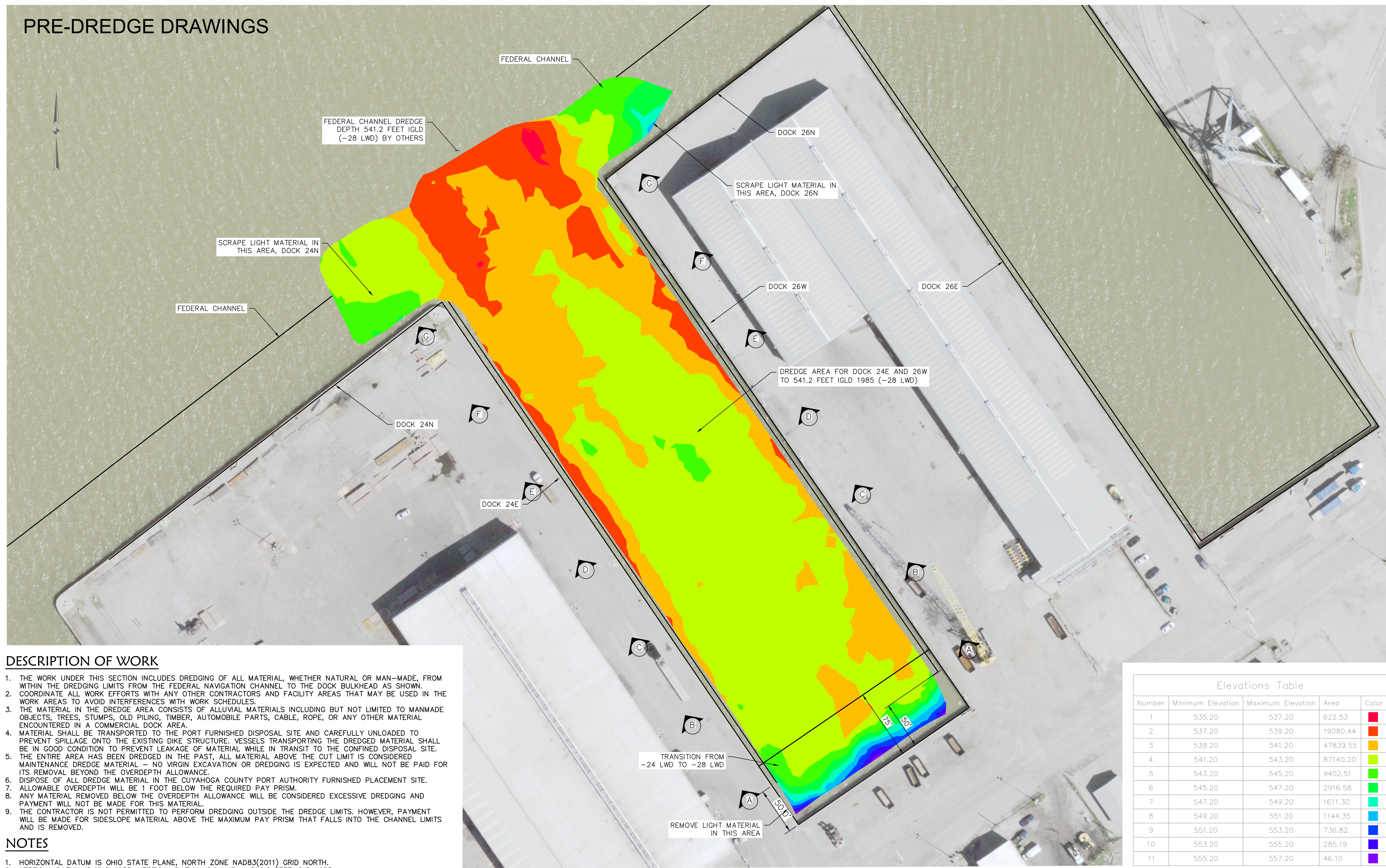
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DATE: 4/12/2024	
DRAWN BY: DAP	
CHK'D BY: MPC	
DWG. NAME: DREDGING	
PATH: CIVIL_30	
F.B.	

KS ASSOCIATES, Inc.
 260 Burns Road, Suite 100
 Elyria, OH 44035
 P 440 365 4730
 F 440 365 4790
 www.ksassociates.com

TITLE SHEET
CLEVELAND HARBOR
DREDGING OPERATIONS
 CLEVELAND PORT AUTHORITY
 1100 WEST 9TH STREET, SUITE #300,
 CLEVELAND, OHIO 44113

SHEET 1	OF 6
JOB NO. 24055	

PRE-DREDGE DRAWINGS



DESCRIPTION OF WORK

1. THE WORK UNDER THIS SECTION INCLUDES DREDGING OF ALL MATERIAL, WHETHER NATURAL OR MAN-MADE, FROM WITHIN THE DREDGING LIMITS FROM THE FEDERAL NAVIGATION CHANNEL TO THE DOCK BULKHEAD AS SHOWN.
2. COORDINATE ALL WORK EFFORTS WITH ANY OTHER CONTRACTORS AND FACILITY AREAS THAT MAY BE USED IN THE WORK AREAS TO AVOID INTERFERENCES WITH WORK SCHEDULES.
3. THE MATERIAL IN THE DREDGE AREA CONSISTS OF ALLUVIAL MATERIALS INCLUDING BUT NOT LIMITED TO MANMADE OBJECTS, TREES, STUMPS, OLD PILING, TIMBER, AUTOMOBILE PARTS, CABLE, ROPE, OR ANY OTHER MATERIAL ENCOUNTERED IN A COMMERCIAL DOCK AREA.
4. MATERIAL SHALL BE TRANSPORTED TO THE PORT FURNISHED DISPOSAL SITE AND CAREFULLY UNLOADED TO PREVENT SPILLAGE ONTO THE EXISTING DIKE STRUCTURE. VESSELS TRANSPORTING THE DREDGED MATERIAL SHALL BE IN GOOD CONDITION TO PREVENT LEAKAGE OF MATERIAL WHILE IN TRANSIT TO THE CONFINED DISPOSAL SITE.
5. THE ENTIRE AREA HAS BEEN DREDGED IN THE PAST, ALL MATERIAL ABOVE THE CUT LIMIT IS CONSIDERED MAINTENANCE DREDGE MATERIAL - NO VIRGIN EXCAVATION OR DREDGING IS EXPECTED AND WILL NOT BE PAID FOR ITS REMOVAL BEYOND THE OVERDEPTH ALLOWANCE.
6. DISPOSE OF ALL DREDGE MATERIAL IN THE CUYAHOGA COUNTY PORT AUTHORITY FURNISHED PLACEMENT SITE.
7. ALLOWABLE OVERDEPTH WILL BE 1 FOOT BELOW THE REQUIRED PAY PRISM.
8. ANY MATERIAL REMOVED BELOW THE OVERDEPTH ALLOWANCE WILL BE CONSIDERED EXCESSIVE DREDGING AND PAYMENT WILL NOT BE MADE FOR THIS MATERIAL.
9. THE CONTRACTOR IS NOT PERMITTED TO PERFORM DREDGING OUTSIDE THE DREDGE LIMITS. HOWEVER, PAYMENT WILL BE MADE FOR SIDESLOPE MATERIAL ABOVE THE MAXIMUM PAY PRISM THAT FALLS INTO THE CHANNEL LIMITS AND IS REMOVED.

NOTES

1. HORIZONTAL DATUM IS OHIO STATE PLANE, NORTH ZONE NAD83(2011) GRID NORTH.
2. VERTICAL DATUM IS IGLD 1985. VERTICAL DATUM: 0.0 LWD = 569.2 FEET IGLD 1985
3. UNITS ARE U.S. SURVEY FEET
4. HYDROGRAPHIC SURVEY PERFORMED BY KS ASSOCIATES ON 3/31/24
5. AERIAL PHOTOGRAPHY FROM OGRIPS DATA DOWNLOADS, OSIP III, NOVEMBER 18, 2017
6. DREDGE LIMITS INCLUDE THE AREA BETWEEN THE FEDERAL CHANNEL AND THE SHEET PILE BULKHEAD.
7. NON-GRANULAR MATERIALS AND DEBRIS SUCH AS STEEL, CONCRETE, ETC., ARE ANTICIPATED TO BE ENCOUNTERED ON THE LAKEBED DURING DREDGING OPERATIONS

Number	Minimum Elevation	Maximum Elevation	Area	Color
1	535.20	537.20	622.53	Red
2	537.20	539.20	19080.44	Orange
3	539.20	541.20	47839.55	Yellow
4	541.20	543.20	87140.20	Light Green
5	543.20	545.20	9402.51	Green
6	545.20	547.20	2916.58	Light Blue
7	547.20	549.20	1611.30	Blue
8	549.20	551.20	1144.35	Dark Blue
9	551.20	553.20	736.82	Very Dark Blue
10	553.20	555.20	285.19	Purple
11	555.20	557.20	46.10	Dark Purple

PRE-DREDGE SITE PLAN



DATE	DESCRIPTION	BY

DATE	DRAWN BY	CHECKED BY	DWG. NAME	SITE PLAN	PATH	F.B.
4/12/2024	DJP	MPC	CIVIL_30			

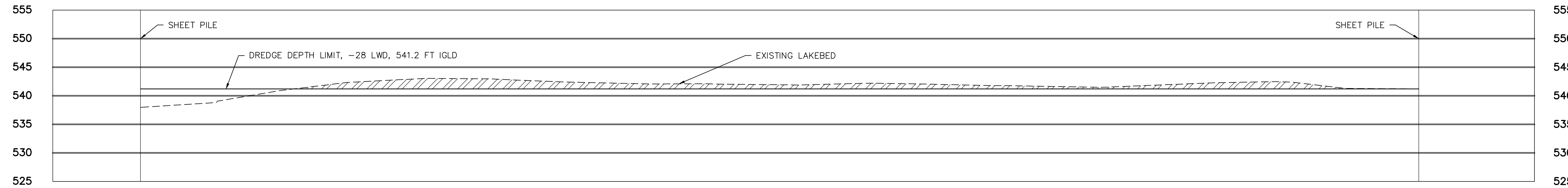
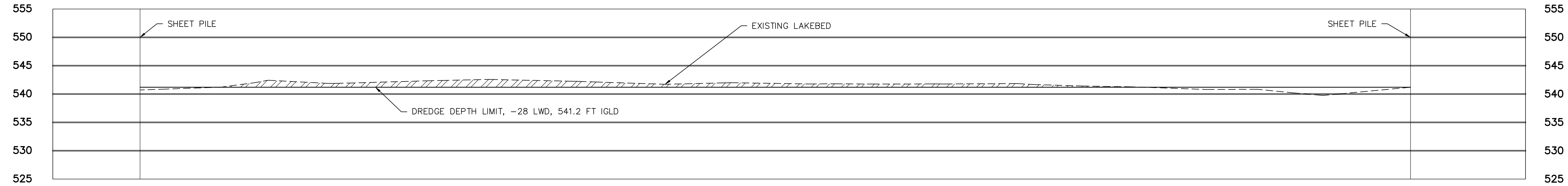
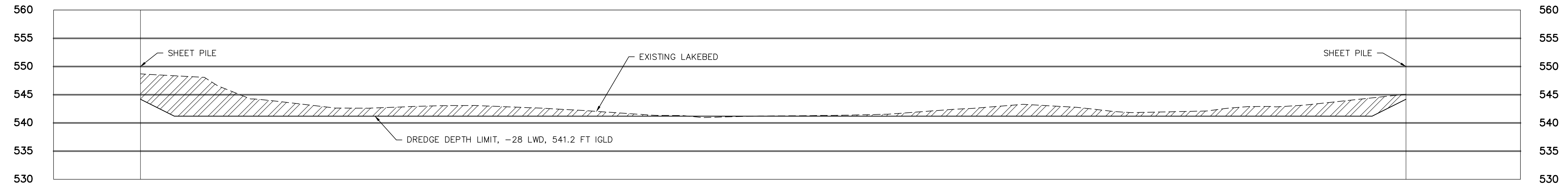
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SITE PLAN
CLEVELAND HARBOR
DREDGING OPERATIONS
 CLEVELAND PORT AUTHORITY
 1100 WEST 9TH STREET, SUITE #300,
 CLEVELAND, OHIO 44113

SHEET	2	OF	6
JOB NO.	24055		

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PRE-DREDGE DRAWINGS



NOTES

1. HORIZONTAL DATUM IS OHIO STATE PLANE, NORTH ZONE NAD83(2011) GRID NORTH.
2. VERTICAL DATUM IS IGLD 1985. VERTICAL DATUM: 0.0 LWD = 569.2 FEET IGLD 1985
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DATE	DESCRIPTION	BY

DATE	DESCRIPTION	BY
4/12/2024	DAP	
	MPC	
	SITE PLAN	
	CIVIL 3D	
	F.B.	

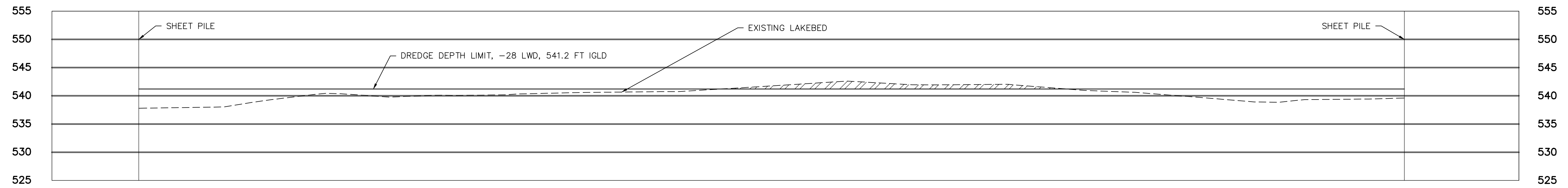
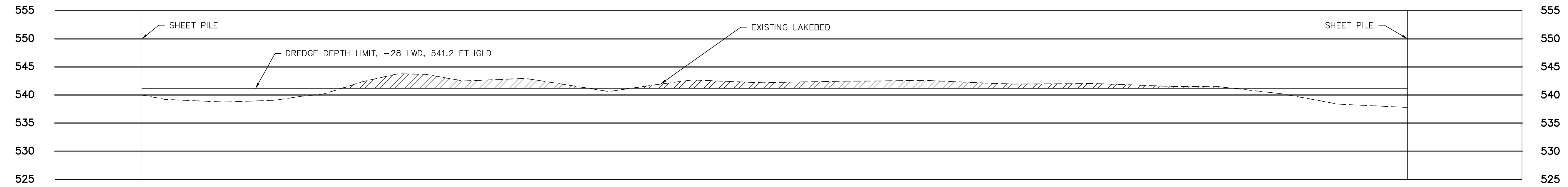
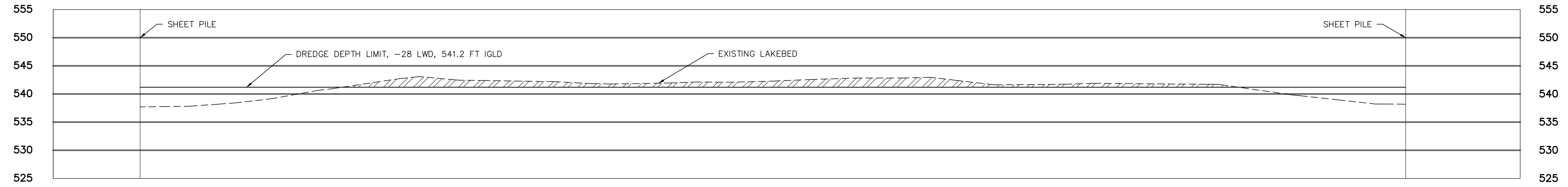
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SECTIONS
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SHEET
4
 OF
6
 JOB NO.
24055

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PRE-DREDGE DRAWINGS



NOTES

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DATE	DESCRIPTION	BY

REVISIONS	
DATE: 4/12/2024	
DRAWN BY: DAP	
CHKD BY: MFC	
DWG. NAME: SITE PLAN	
PATH: CIVIL_3D	
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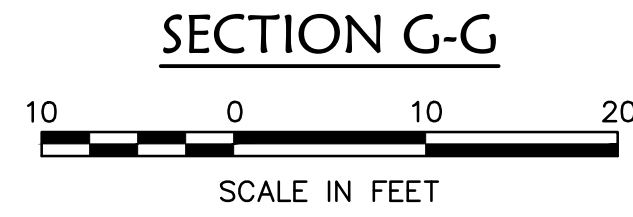
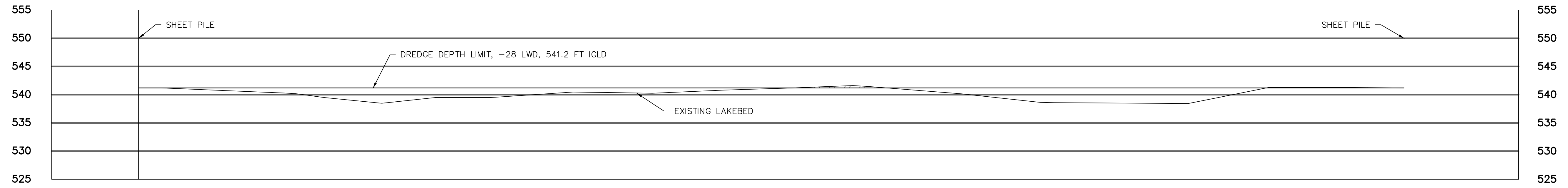
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SHEET	5
OF	6
JOB NO.	24055

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PRE-DREDGE DRAWINGS



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 CLEVELAND, OHIO 44113

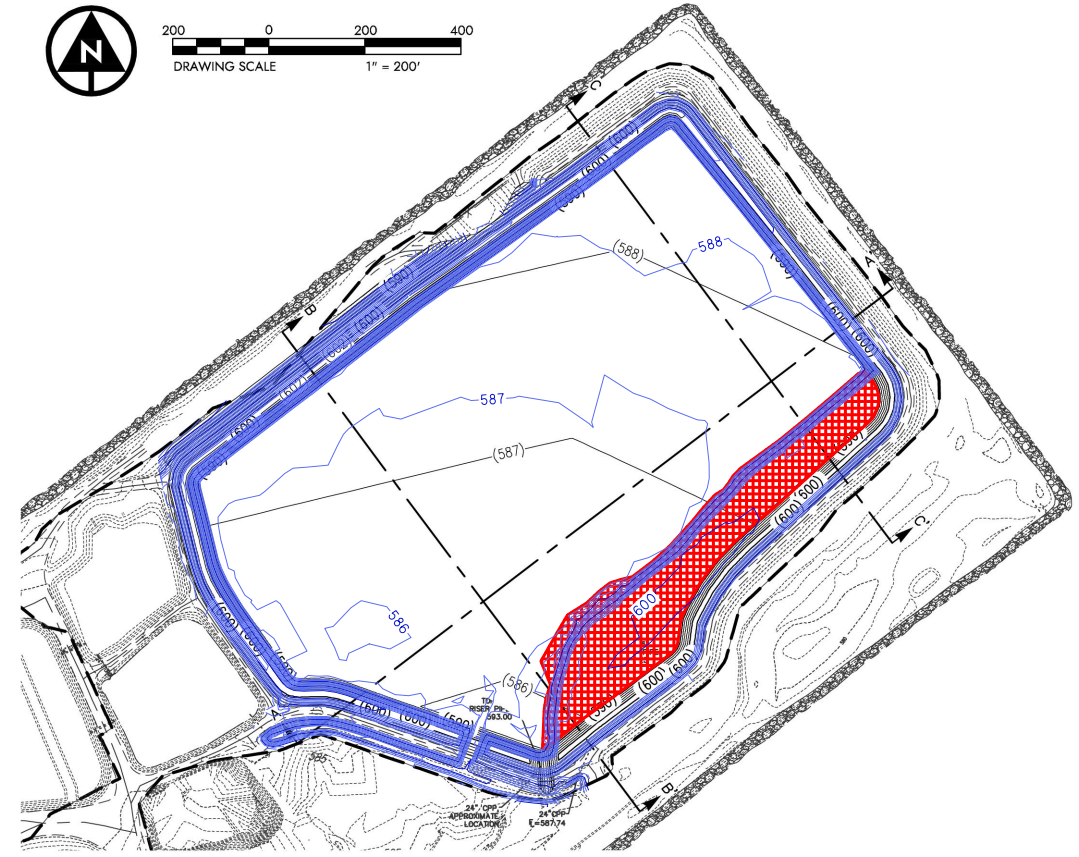
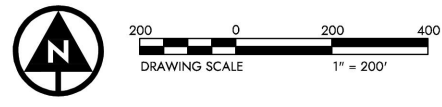
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 24055

REVISIONS

DATE	DESCRIPTION	BY

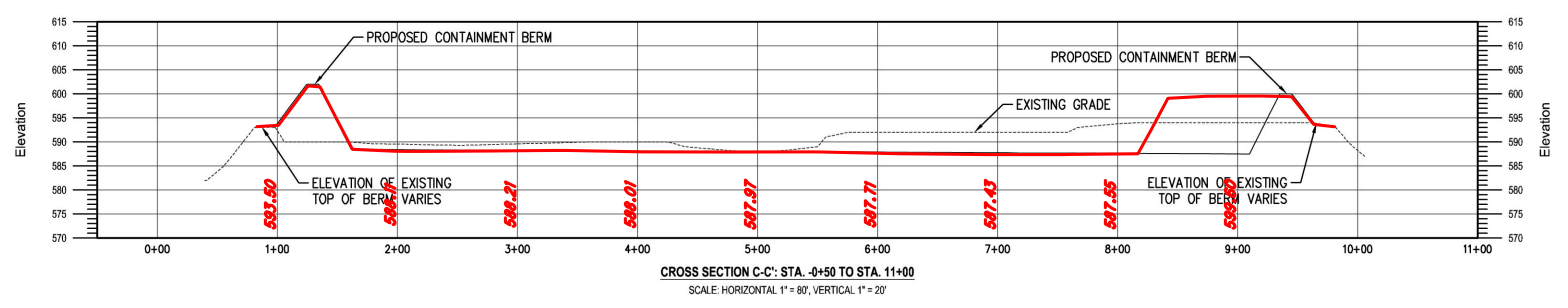
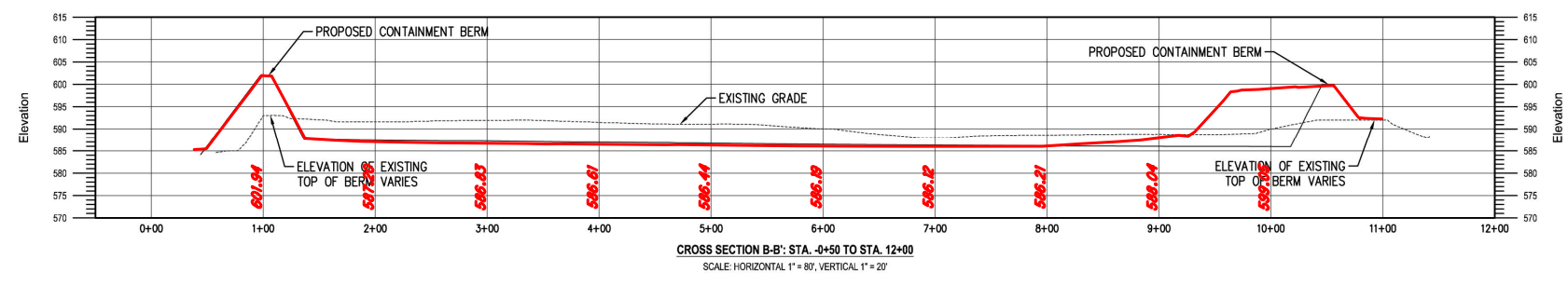
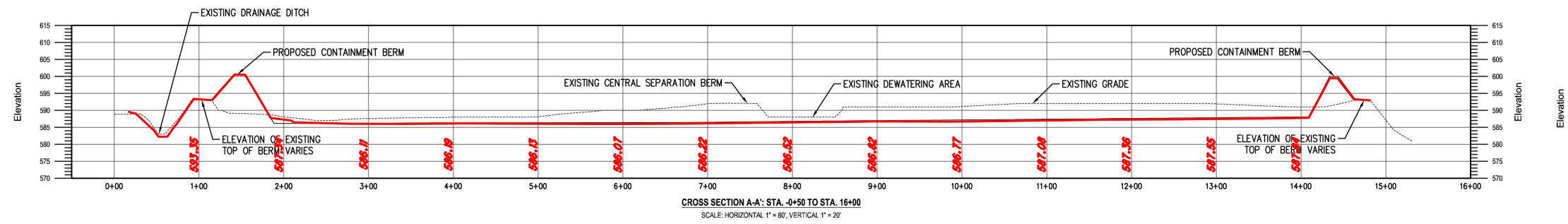
DRAFT

EXHIBIT B



- NOTES:
- EXISTING TOPOGRAPHY WITHIN IS BASED ON FIELD SURVEY COMPLETED BY GARCIA SURVEYORS, INC. ON 11/16/2022 USING OHIO STATE PLANE COORDINATE SYSTEM, NORTH ZONE, NAD83, AND VERTICAL DATUM NAVD88.
 - EXISTING SITE CONDITIONS MAY VARY FROM CONDITIONS AND GRADES SHOWN ON THESE PLANS DUE TO ON-GOING SITE OPERATOR'S ACTIVITIES, SOIL STOCKPILING, DREDGING ACTIVITIES, CONSOLIDATION OF PLACED MATERIALS AND OTHER TOPOGRAPHY CHANGES TYPICAL TO A DYNAMIC FACILITY.

AS-BUILT SURVEY PERFORMED BY:
DAVEY **Davey Resource Group**
 1310 SHARON COPLEY ROAD, P.O. BOX 37
 SHARON-CENTER, OHIO 44274
 (PHONE) 330.590.8004 (FAX) 888.820.8423



CLEVELAND ~ CUYAHOGA COUNTY ~ OHIO
PORT OF CLEVELAND 2024-2025
MECHANICAL CAPACITY
CONTAINMENT BERMS
 PROPOSED CDF 12 CROSS-SECTIONS

APPLICATION NO.
 DATE: 3/17/2023

AS NOTED

9.0



verdantas
 PEOPLE FOCUSED FUTURE
 219 SOUTH ERIE STREET
 TOLEDO, OHIO 43604-8607
 TEL 419.385.2018
 FAX 614.360.0023



DESIGNED BY: SIC	DRAWN BY: SIC	CHECKED BY: DIP/CLF	PROJECT NO. 16205
CHK'D BY: DATE	REVISION	No.	

Z:\project files\1600016205\radial sheets and pans\phase 6 and 7\file-16205-cross sections.dwg

DRAFT

EXHIBIT C



DEPARTMENT OF THE ARMY
BUFFALO DISTRICT, CORPS OF ENGINEERS
1776 NIAGARA STREET
BUFFALO, NEW YORK 14207-3199

**PLEASE SIGN AND SUBMIT
THE COMPLETION FORM
ATTACHED TO THIS PERMIT**

REPLY TO
ATTENTION OF:

November 2, 2017

Regulatory Branch

SUBJECT: Letter of Permission - Processing No. 2007-00146.

Mr. William Friedman
Cleveland-Cuyahoga County Port Authority
1100 W. 9th Street, Suite 300
Cleveland, Ohio 44113

Dear Mr. Friedman:

Please refer to your request for a Department of the Army (DA) permit to perform maintenance dredging in the Cuyahoga River below the Lake Erie ordinary high water mark (OHWM) of 573.4 International Great Lakes Datum (IGLD), 1985, located at the Port of Cleveland's General Cargo Facility commercial general cargo Docks 20, 22, 24, 26, and 28 adjacent to the Port of Cleveland's west basin at 775 Erieside Avenue, in the City of Cleveland, Cuyahoga County, Ohio.

UPON RECOMMENDATION OF THE CHIEF OF ENGINEERS, PURSUANT TO SECTION 10 OF THE RIVERS AND HARBORS ACT OF 3 MARCH 1899 (33 U.S.C. 403) AND SECTION 404 OF THE CLEAN WATER ACT (33 U.S.C. 1344), CLEVELAND-CUYAHOGA COUNTY PORT AUTHORITY, DESIGNATED THE PERMITTEE, IS HEREBY GRANTED PERMISSION TO MAINTENANCE DREDGE APPROXIMATELY 50,000 CUBIC YARDS OF SEDIMENT FROM THE CUYAHOGA RIVER BELOW THE LAKE ERIE ORDINARY HIGH WATER MARK (OHWM) OF 573.4 INTERNATIONAL GREAT LAKES DATUM (IGLD), 1985, OVER APPROXIMATELY FOURTEEN (14) ACRES AND TO A DREDGE DEPTH OF 27' BELOW THE LOW WATER DATUM TO MATCH THE CLEVELAND HARBOR DREDGE DEPTH OF APPROXIMATELY 542.2' IGLD, 1985, AND DISPOSE OF THIS MATERIAL IN THE PORT OF CLEVELAND'S CONFINED DISPOSAL FACILITIES 9 AND 12 IN ACCORDANCE WITH THE ATTACHED PERMIT PROVISIONS, CONDITIONS AND DRAWINGS WHICH ARE INCORPORATED IN AND MADE A PART HEREOF.

PLEASE NOTE: The term "you" and its derivatives, as used in this permit, means the permittee or any future transferee. The term "this office" refers to the appropriate district or division office of the Corps of Engineers having jurisdiction over the permitted activity under the authority of the commanding officer.

Regulatory Branch

SUBJECT: Letter of Permission - Processing No. 2007-00146

This letter is an initial proffered permit for your proposed project. If you object to this decision, you may request an administrative appeal under Corps regulations at 33 CFR Part 331. Enclosed you will find a Notification of Appeal Process (NAP) fact sheet and Request for Appeal (RFA) form. If you request to appeal the above decision, you must submit a completed RFA form to our office within 60 days of the date on this letter.

In order for an RFA to be accepted by the Corps, the Corps must determine that it is complete; that it meets the criteria for appeal under 33 C.F.R. part 331.5, and that it has been received by the District Office within 60 days of the date of the NAP. Should you decide to submit an RFA form, it must be received at the above address by January 2, 2017.

It is not necessary to submit an RFA if you do not object to the decision in this letter.

The District Commander must be informed of commencement and completion of the authorized work. Please use the forms enclosed. Commencement indicates your acceptance and agreement to comply with the permit terms and conditions.

Questions pertaining to this matter should be directed to Melissa Tarasiewicz, who may be contacted by calling 716-879-4159, by writing to the following address: U.S. Army Corps of Engineers, 1776 Niagara Street, Buffalo, New York, 14207, or by e-mail at: melissa.j.tarasiewicz@usace.army.mil

Dated this 2nd day of November 2017.

BY AUTHORITY OF THE SECRETARY OF THE ARMY:


for Diane C. Kozlowski
Chief, Regulatory Branch

Enclosures

PERMIT CONDITIONS

GENERAL CONDITIONS:

1. The time limit for completing the work authorized ends on November 2, 2027. If you find that you need more time to complete the authorized activity, submit your request for a time extension to this office for consideration at least one month before the above date is reached.
2. You must maintain the activity authorized by this permit in good condition and in conformance with the terms and conditions of this permit. You are not relieved of this requirement if you abandon the permitted activity, although you must make a good faith transfer to a third party in compliance with General Condition 4 below. Should you wish to cease to maintain the authorized activity, or should you desire to abandon it without a good faith transfer, you may obtain a modification of this permit from this office, which may require restoration of the area.
3. If you discover any previously unknown historic or archaeological remains while accomplishing the activity authorized by this permit, you must immediately notify this office of what you have found. We will initiate the Federal and state coordination required to determine if the remains warrant a recovery effort or if the site is eligible for listing in the National Register of Historic Places.
4. If you sell the property associated with this permit, you must obtain the signature of the new owner in the space provided and forward a copy of the permit to this office to validate the transfer of this authorization.
5. If a conditioned water quality certification has been issued for your project, you must comply with the conditions specified in the certification as special conditions to this permit.
6. You must allow representatives from this office to inspect the authorized activity at any time deemed necessary to ensure that it is being or has been accomplished in accordance with the terms and conditions of your permit.
7. The permittee understands and agrees that, if future operations by the United States require the removal, relocation, or other alteration, of the structure or work herein authorized, or if, in the opinion of the Secretary of the Army or his authorized representative, said structure or work shall cause unreasonable obstruction to the free navigation of the navigable waters, the permittee will be required, upon due notice from the Corps of Engineers, to remove, relocate, or alter the structural work or obstructions caused thereby, without expense to the United States. No claim shall be made against the United States on account of any such removal or alteration.

FURTHER INFORMATION:

1. Congressional Authorities: You have been authorized to undertake the activity described above pursuant to:

SECTION 10 OF THE RIVERS AND HARBORS ACT OF 3 MARCH 1899 (33 U.S.C. 403)

2. Limits of this authorization.

- a. This permit does not obviate the need to obtain other Federal, state or local authorizations required by law.
- b. This permit does not grant any property rights or exclusive privileges.
- c. This permit does not authorize any injury to the property or rights of others.
- d. This permit does not authorize interference with any existing or proposed Federal project.

3. Limits of Federal Liability. In issuing this permit, the Federal Government does not assume any liability for the following:

- a. Damages to the permitted project or uses thereof as a result of other permitted or unpermitted activities or from natural causes.
- b. Damages to the permitted project or uses thereof as a result of current or future activities undertaken by or on behalf of the United States in the public interest.
- c. Damages to persons, property, or to other permitted or unpermitted activities or structures caused by the activity authorized by this permit.
- d. Design or construction deficiencies associated with the permitted work.
- e. Damage claims associated with any future modification, suspension, or revocation of this permit.

4. Reliance on Applicant's Data: The determination of this office that issuance of this permit is not contrary to the public interest was made in reliance on the information you provided.

5. Reevaluation of Permit Decision. This office may reevaluate its decision on this permit at any time the circumstances warrant. Circumstances that could require a reevaluation include, but are not limited to, the following:

- a. You fail to comply with the terms and conditions of this permit.
- b. The information provided by you in support of your permit application proves to have been false, incomplete, or inaccurate (see 4 above).
- c. Significant new information surfaces which this office did not consider in reaching the original public interest decision.

Such a reevaluation may result in a determination that it is appropriate to use the suspension, modification, and revocation procedures contained in 33 CFR 325.7 or enforcement procedures such as those contained in 33 CFR 326.4 and 326.5. The referenced enforcement procedures provide for the issuance of an administrative order requiring you to comply with the terms and conditions of your permit and for the initiation of legal action where appropriate. You will be required to pay for any corrective measures ordered by this office, and if you fail to comply with such directive, this office may in certain situations (such as this specified in 33 CFR 209.170) accomplish the corrective measures by contract or otherwise and bill you for the cost.

6. Extensions. General condition 1 establishes a time limit for the completion of the activity authorized by this permit. Unless there are circumstances requiring either a prompt completion of the authorized activity or a reevaluation of the public interest decision, the Corps will normally give favorable consideration to a request for an extension of this time limit.

SPECIAL CONDITIONS:

1. That no in-water work shall be performed between March 15 and June 30 to preclude adverse impacts on the spawning, nursery, and feeding activities of indigenous fish species. For 2018 only, a waiver of the in-water work restricted period is authorized. This applies only to this project and this waiver expires June 30, 2018. If the in-water work is not completed during this authorized period, and in-water work during the restricted period is necessary, you must request an additional waiver of the in-water work restriction period. This request shall be submitted to the attention of: Melissa Tarasiewicz, U.S. Army Corps of Engineers, 1776 Niagara Street, Buffalo, NY 14207 and must be coordinated with the Ohio Department of Natural Resources- Division of Wildlife. To the greatest extent practicable any in-water work should be completed as soon as possible and avoided between March 15 and June 30 to preclude adverse impacts on the spawning, nursery, and feeding activities of indigenous fish species.
2. That you shall perform the work authorized by this permit in such a way that it will not interfere with any scheduled or ongoing maintenance activity conducted by the Federal Government. Information regarding the scheduling of Federal maintenance projects may be obtained by contacting Mr. Bob Remmers, Chief, Operations and Technical Support Section, U.S. Army Corps of Engineers, Buffalo District, 1776 Niagara Street, Buffalo, New York 14207-3199.
3. You are responsible for ensuring that all contractors and/or workers executing the activity(s) authorized by this permit have knowledge of the terms and conditions of the authorization and that a copy of the permit document is on the vessel used for the authorized transportation and disposal of dredged material throughout the period that the authorized work is underway.
4. The permittee, including their contractors, must ensure the dredged material is not temporarily or permanently placed in Waters of the U.S., including wetlands.

5. The permittee must only dispose of dredged material in the Port of Cleveland's Confined Disposal Facilities 9 and 12.
6. That under no circumstances shall you allow any dredged material, or water containing dredged material, to be spilled or otherwise discharged from the barge enroute to or from the Confined Disposal Facility.
7. Maintenance dredging under this authorization shall not be extended beyond the maximum period of ten (10) years. A new permit application will need to be submitted for dredging activities beyond this period. It is recommend to submit any time extension request to this office at least three (3) months prior to the expiration date of the initial five-year period.

TRANSFeree HEREBY AGREES TO COMPLY WITH
THE TERMS AND CONDITIONS OF THIS PERMIT.*

Transferee

Date

*Note: When the structures or work authorized by this permit are still in existence at the time the property is transferred, the terms and conditions of this permit will continue to be binding on the new owner(s) of the property. To validate the transfer of this permit and the associated liabilities associated with compliance with its terms and conditions, have the transferee sign and date in the space provided. A copy of this signed permit and statement shall be forwarded to the Buffalo District at the following address:

U.S. Army Corps of Engineers
Buffalo District
Regulatory Branch
1776 Niagara Street
Buffalo, New York 14207

NOTIFICATION OF ADMINISTRATIVE APPEAL OPTIONS AND PROCESS AND REGISTERED APPEAL

Applicant: Cleveland-Cuyahoga County Port Authority		File Number: 2007-00146	Date: 11/2/2017
Attached is:			See Section below
X	INITIAL PROFFERED PERMIT (Standard Permit or Letter of permission)	A	
	PROFFERED PERMIT (Standard Permit or Letter of permission)	B	
	PERMIT DENIAL	C	
	APPROVED JURISDICTIONAL DETERMINATION	D	
	PRELIMINARY JURISDICTIONAL DETERMINATION	E	

SECTION I: The following identifies your rights and options regarding an administrative appeal of the above decision. Additional information may be found at http://www.usace.army.mil/CECW/Pages/reg_materials.aspx or Corps regulations at 33 CFR Part 331.

- A: INITIAL PROFFERED PERMIT:** You may accept or object to the permit.
- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
 - **OBJECT:** If you object to the permit (Standard or LOP) because of certain terms and conditions therein, you may request that the permit be modified accordingly. You must complete Section II of this form and return the form to the district engineer. Your objections must be received by the district engineer within 60 days of the date of this notice, or you will forfeit your right to appeal the permit in the future. Upon receipt of your letter, the district engineer will evaluate your objections and may: (a) modify the permit to address all of your concerns, (b) modify the permit to address some of your objections, or (c) not modify the permit having determined that the permit should be issued as previously written. After evaluating your objections, the district engineer will send you a proffered permit for your reconsideration, as indicated in Section B below.
- B: PROFFERED PERMIT:** You may accept or appeal the permit
- **ACCEPT:** If you received a Standard Permit, you may sign the permit document and return it to the district engineer for final authorization. If you received a Letter of Permission (LOP), you may accept the LOP and your work is authorized. Your signature on the Standard Permit or acceptance of the LOP means that you accept the permit in its entirety, and waive all rights to appeal the permit, including its terms and conditions, and approved jurisdictional determinations associated with the permit.
 - **APPEAL:** If you choose to decline the proffered permit (Standard or LOP) because of certain terms and conditions therein, you may appeal the declined permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.
- C: PERMIT DENIAL:** You may appeal the denial of a permit under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.
- D: APPROVED JURISDICTIONAL DETERMINATION:** You may accept or appeal the approved JD or provide new information.
- **ACCEPT:** You do not need to notify the Corps to accept an approved JD. Failure to notify the Corps within 60 days of the date of this notice, means that you accept the approved JD in its entirety, and waive all rights to appeal the approved JD.
 - **APPEAL:** If you disagree with the approved JD, you may appeal the approved JD under the Corps of Engineers Administrative Appeal Process by completing Section II of this form and sending the form to the division engineer. This form must be received by the division engineer within 60 days of the date of this notice.
- E: PRELIMINARY JURISDICTIONAL DETERMINATION:** You do not need to respond to the Corps regarding the preliminary JD. The Preliminary JD is not appealable. If you wish, you may request an approved JD (which may be appealed), by contacting the Corps district for further instruction. Also you may provide new information for further consideration by the Corps to reevaluate the JD.

SECTION II - REQUEST FOR APPEAL or OBJECTIONS TO AN INITIAL PROFFERED PERMIT

REASONS FOR APPEAL OR OBJECTIONS: (Describe your reasons for appealing the decision or your objections to an initial proffered permit in clear concise statements. You may attach additional information to this form to clarify where your reasons or objections are addressed in the administrative record.)

ADDITIONAL INFORMATION: The appeal is limited to a review of the administrative record, the Corps memorandum for the record of the appeal conference or meeting, and any supplemental information that the review officer has determined is needed to clarify the administrative record. Neither the appellant nor the Corps may add new information or analyses to the record. However, you may provide additional information to clarify the location of information that is already in the administrative record.

POINT OF CONTACT FOR QUESTIONS OR INFORMATION:

If you have questions regarding this decision and/or the appeal process you may contact:

Melissa Tarasiewicz
United States Army Corps of Engineers
Buffalo District
1776 Niagara Street
Buffalo, NY 14207
716-879-4159
melissa.j.tarasiewicz@usace.army.mil

If you only have questions regarding the appeal process you may also contact:

Jacob Siegrist
Great Lakes and Ohio River Division
CELRD-PDS-O
550 Main Street, Room 10524
Cincinnati, OH 45202-3222
(513) 684-2699;FAX(513) 684-2460

RIGHT OF ENTRY: Your signature below grants the right of entry to Corps of Engineers personnel, and any government consultants, to conduct investigations of the project site during the course of the appeal process. You will be provided a 15 day notice of any site investigation, and will have the opportunity to participate in all site investigations.

Signature of appellant or agent.

Date:

Telephone number:

IMPORTANT

This form must be completed and mailed to the District Commander at: Regulatory Branch, US Army Corps of Engineers, 1776 Niagara Street, Buffalo, New York 14207, prior to commencement of any work authorized by Department of the Army Permit No. 2007-00146 (Letter of Permission).

Date:

City of Cleveland
Cuyahoga County
Ohio

Mr. David Leput
Regulatory Branch
U.S. Army Corps of Engineers
1776 Niagara Street
Buffalo, New York 14207

Dear Mr. Leput:

You are hereby notified that the work authorized under Department of the Army Permit No. 2007-00146, issued to Cleveland-Cuyahoga County Port Authority, to perform maintenance dredging in the Cuyahoga River below the Lake Erie ordinary high water mark (OHWM) of 573.4 International Great Lakes Datum (IGLD), 1985, located at the Port of Cleveland's General Cargo Facility commercial general cargo Docks 20, 22, 24, 26, and 28 adjacent to the Port of Cleveland's west basin at 775 Erieside Avenue, in the City of Cleveland, Cuyahoga County, Ohio, will be started on or about _____ (Month/Day/Year).

The first work to be undertaken is as follows: _____

In commencing the work, I accept and agree to comply with the terms and conditions of the permit.

By:

(Authorized Signature)

(Title)

Date: _____

Permittee Telephone Number: _____

File Closed: 11/2/2017

IMPORTANT

This form must be completed and mailed to the District Commander at: Regulatory Branch, US Army Corps of Engineers, 1776 Niagara Street, Buffalo, New York 14207, immediately upon completion of work authorized by Department of the Army Permit No. 2007-00146 (Letter of Permission).

Date:

City of Cleveland
Cuyahoga County
Ohio

Mr. David Leput
Regulatory Branch
U.S. Army Corps of Engineers
1776 Niagara Street
Buffalo, New York 14207

Dear Mr. Leput:

You are hereby notified that the work authorized under Department of the Army Permit No. 2007-00146, issued to Cleveland-Cuyahoga County Port Authority, to perform maintenance dredging in the Cuyahoga River below the Lake Erie ordinary high water mark (OHWM) of 573.4 International Great Lakes Datum (IGLD), 1985, located at the Port of Cleveland's General Cargo Facility commercial general cargo Docks 20, 22, 24, 26, and 28 adjacent to the Port of Cleveland's west basin at 775 Erieside Avenue, in the City of Cleveland, Cuyahoga County, Ohio, was (completed/discontinued) on _____ Month/Day/Year).

If Discontinued:

The work is _____ percent complete. The following remains to be done before all work authorized by this permit shall have been completed:

By: _____ Date: _____
(Authorized Signature) (Title)

Permittee Telephone Number: _____

File Closed: 11/2/2017

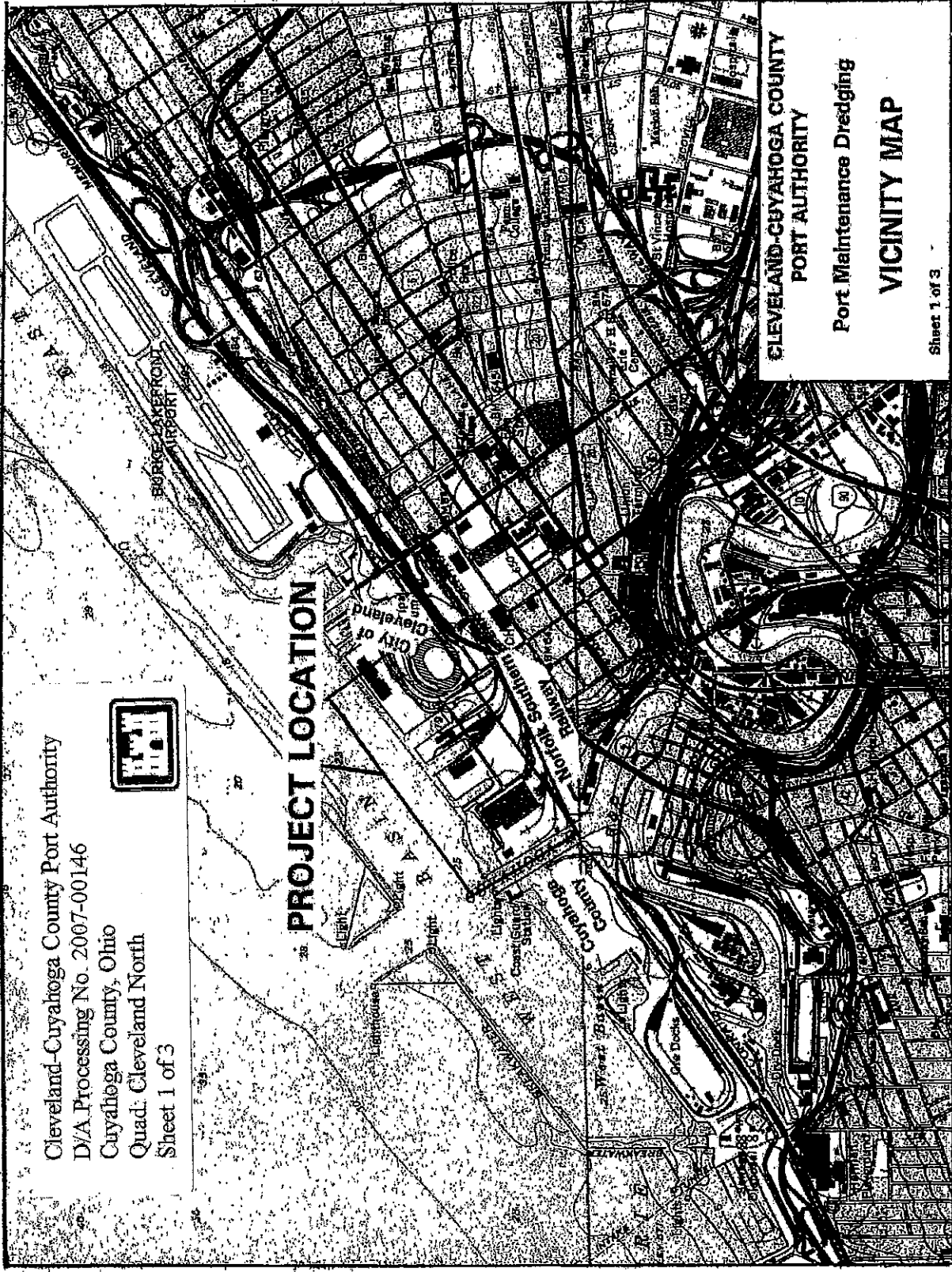
44.5061 81.7053

Cleveland North USGS

Cleveland-Cuyahoga County Port Authority
D/A Processing No. 2007-00146
Cuyahoga County, Ohio
Quad: Cleveland North
Sheet 1 of 3



PROJECT LOCATION



CLEVELAND-CUYAHOGA COUNTY
PORT AUTHORITY

Port Maintenance Dredging

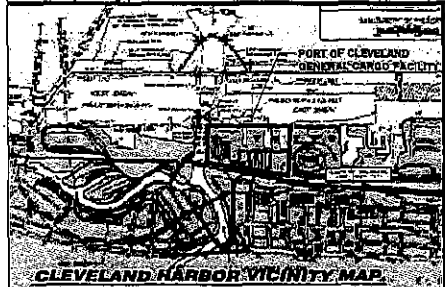
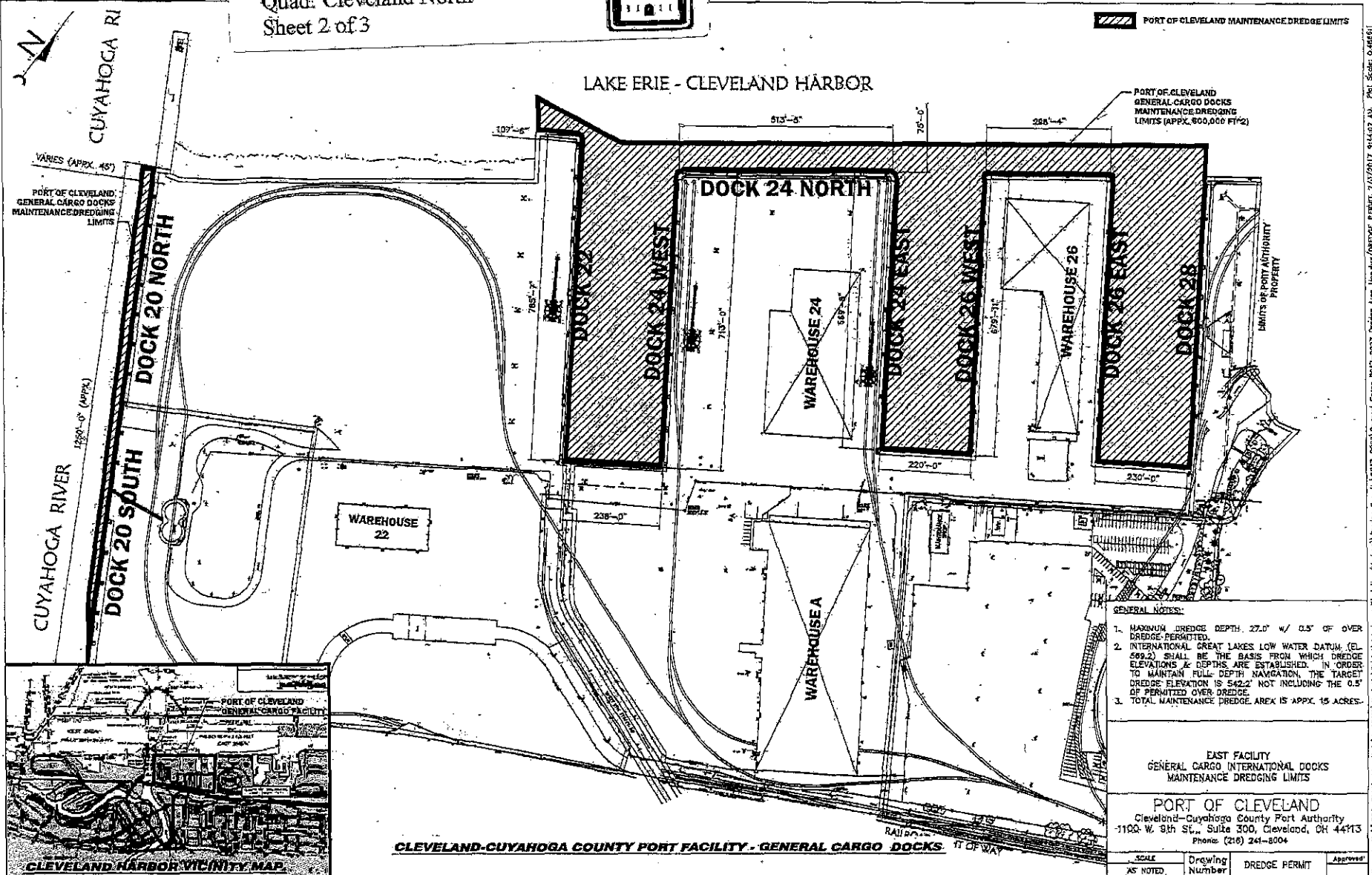
VICINITY MAP

Sheet 1 of 3

Cleveland-Cuyahoga County Port Authority
 D/A Processing No. 2007-00146
 Cuyahoga County, Ohio
 Quad: Cleveland North
 Sheet 2 of 3



LAKE ERIE - CLEVELAND HARBOR



- GENERAL NOTES:
1. MAXIMUM DREDGE DEPTH 27.0' w/ 0.5' OF OVER DREDGE PERMITTED.
 2. INTERNATIONAL GREAT LAKES LOW WATER DATUM (EL. 569.2) SHALL BE THE BASIS FROM WHICH DREDGE ELEVATIONS & DEPTHS ARE ESTABLISHED. IN ORDER TO MAINTAIN FULL DEPTH NAVIGATION, THE TARGET DREDGE ELEVATION IS 542.2' NOT INCLUDING THE 0.5' OF PERMITTED OVER DREDGE.
 3. TOTAL MAINTENANCE DREDGE AREA IS APPX. 15 ACRES.

EAST FACILITY
 GENERAL CARGO INTERNATIONAL DOCKS
 MAINTENANCE DREDGING LIMITS

PORT OF CLEVELAND
 Cleveland-Cuyahoga County Port Authority
 1100 W. 9th St., Suite 300, Cleveland, OH 44113
 Phone (216) 241-8004

SCALE	Drawing Number	DREDGE PERMIT	Approval
AS NOTED			

11/07/07 11:17, Printed by: Richard Updegraff from 11/07/07 11:17, PLOT SCALE 0.44611
 7/17/2017 9:44:02 AM, Plot Scale 0.44611
 2017-2022 Dredge Area Layout/PERMITS/7/17/2017 9:44:02 AM, Plot Scale 0.44611

Cleveland-Cuyahoga County Port Authority
D/A Processing No. 2007-00146
Cuyahoga County, Ohio
Quad: Cleveland North
Sheet 3 of 3



Top of Dock

Existing Steel
Sheet Pile Bulkhead

LWD (EL. 569.2')

Varies

Material to be removed

-27.0' LWD
(EL. 542.2')
EXCLUDES OVER DREDGE

CLEVELAND-CUYAHOGA COUNTY
PORT AUTHORITY

Port Maintenance Dredging

TYPICAL SECTION

Sheet 3 of 3

DRAFT

EXHIBIT D



CITY OF CLEVELAND
Mayor Frank G. Jackson

INBOUND



OUTBOUND



2023-2024 SEDIMENT PROCESSING & MANAGEMENT FACILITY

SITE OPERATIONS PLAN

DREDGE MATERIAL PLACEMENT &
HYDRAULICALLY DELIVERED DREDGE SEDIMENT PROCESSING & RECYCLING

CONSTRUCTION SAFETY & PHASING PLAN

September 13, 2022

PREFACE: FAA EVALUATION CONSIDERATION

The Cleveland-Cuyahoga County Port Authority does not propose making any changes to existing site operations and dredge placement activities or to the overall site footprint. This CSPP and associated 7460 application are for the extension and/or renewal of the Port Authority's current CSPP and 7460 determination, granted on November 9, 2021 and expiring on May 9, 2023.

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Facility Overview

The Cleveland-Cuyahoga County Port Authority (“Port Authority”) operates a dredge sediment repository north of Burke Lakefront Airport (BKL) on a series of confined disposal facilities (“CDFs”) known as the Port of Cleveland’s Sediment Processing & Management Facility (“Facility”). This dredge repository is critical to the region as it is the only Facility permitted to receive the 250,000 CY of sediment that is dredged annually from Cleveland Harbor and the Cuyahoga River by the United States Army Corp. of Engineers (“USACE”). From this Facility, the Port Authority beneficially harvests, recycles, and removes approximately 160,000 CY of dredge sediment from the Facility. The beneficial reuse and recycling operations are critical to maintaining the viability of the facilities long term as it regenerates and preserves capacity for the Federal government and the fulfillment of their mission. The Federal government’s biannual dredging of the Cuyahoga River and the 10,000+ jobs connected to the maritime industry in the region depend on the continuous and uninterrupted operation of this Facility.

The Port Authority operates the Sediment Processing & Management Facility on the northern half of CDF 9 & all areas of CDF 12. The map below highlights the locations of the CDFs relative to Burke Lakefront Airport. The Cleveland-Cuyahoga County Port Authority operates the CDFs under the Harbor Service Agreement with the City of Cleveland established in 2012. This Facility is being considered as an integral part to the USACE’s Dredge Material Management Plan for Cleveland Harbor, which serves as the Federal government’s plan for the long-term handling of dredge sediment in Cleveland Harbor through 2038.



CONFINED DISPOSAL FACILITIES NW BURKE LAKEFRONT AIRPORT

General

This Construction Safety & Phasing Plan was prepared to meet the requirements of FAA Advisory Circular 150/5370-2G, “Operational Safety on Airports During Construction”. It is a standalone document written to establish safety and security controls for performing the work under this Project. The Project Area is shown in the above diagram, inside the yellow line and further detailed in the attached Appendices. This CSPP submission is intended to cover the annual operations at the Facility that includes the 2023 and 2024 inbound placement of approximately 250,000+ CY of dredge sediment into the Facility and the dewatering, recycling, and removal of approximately 160,000 CY of this sediment via outbound truck movements from the Facility. This CSPP shall also cover routine site maintenance operations which could include haul route maintenance, site mowing/vegetation control, & engineering/survey inspections.

2023-2024 CONSTRUCTION SAFETY & PHASING PLAN: ANNUAL SITE OPERATIONS

Major capital improvement work linked to this Facility is excluded from this CSPP. Separate, standalone CSPPs will be submitted in advance of any major capital improvement work occurring. Future capital investments include a multiyear plan to construct required berm containment capacity on CDF 12 and improvements to the access road to decrease fugitive dust emissions and material tracked out onto N. Marginal Rd. Both projects' CSPPs and 7460s will be resubmitted in the Fall of 2022.

This CSPP covers the inbound placement of dredge sediment and the outbound movement of beneficially harvested and recycled dredge sediment at the Facility. The inbound placement of dredge sediment by the USACE's dredging contractor occurs bi-annually over a 6-8-week scheduled duration in May/June and then again in October/November. The inbound placement of dredge sediment occurs on a 24-7 calendar. The dewatering, harvesting, stockpiling, and outbound movement of dredge sediment occurs all year long, Monday-Friday, 7am to 4pm with outbound truck traffic business limited from 7am to 3pm unless special arrangements are made in advance. This CSPP covers the actions and responsibilities of contractors, inspectors, service providers, and Port Authority personnel tied to the inbound placement and outbound movement of dredge sediment at the Facility and any associated maintenance activities that must occur in support of this operation.

In the event the contractor's actions are found to be non-compliant with the requirements of the CSPP or SPCD, the Airport's representatives will direct the Port Authority in writing to immediately stop all operations of that particular work until such time all deficiencies are mitigated and/or corrected to the satisfaction of the Airport and the FAA.

The CSPP and SPCD will be available at all times on the jobsite and shall be held by every contractor performing work and the Port Authority's Site Security and Access Control Team at the Facility under this CSPP. It is the responsibility of the contractor to ensure all construction personnel are familiar with the safety procedures and regulations of the Airport.

Contractors will be required to sign and submit an updated "Safety Plan Compliance Document" (SPCD) consistent with this updated CSPP for the 2023 and 2024 operation years (Appendix A). Within the content of the SPCD, the Contractors will include a statement that they have read and understand the CSPP and detail how they will comply with all the requirements and safety procedures included in this CSPP. Any information not discussed in the original CSPP or ANY changes to the Project MUST be outlined in the SPCD and submitted to the Airport for review and approval. The Airport and the FAA must approve these modifications prior to commencing any work that varies from this plan.

Critical Points of Contact & Coordination

Meetings with BKL personnel to discuss the Project, scope, and schedule related to our site operations are presently held on a bi-weekly basis and meeting minutes are recorded by the Port Authority and distributed to stakeholders. Two weeks prior to the start of the inbound placement of dredge sediment into the Facility, a notification will be sent to Airport personnel outlining the Project schedule, a list of personnel that will be on site, Contractor's SPCD

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documents, and a description of the work activities will also be submitted. A list of critical points of contact for the duration of this CSPP have been included in the chart below. Space holders have been left for the dredging contractor, as the USACE has not bid 2023 and 2024 dredge cycle contracts at this time.

AGENCY	NAME	TITLE	PHONE
Cleveland-Cuyahoga County Port Authority	Nicholas LaPointe	Director, Planning & Capital Development	419-349-7553
Cleveland-Cuyahoga County Port Authority	Carly Beck	GIS/Environmental Specialist	419-386-6095
City of Cleveland-Cleveland Airport System	Anthony Campofredano	Airport Maintenance Supervisor	216-781-6411
Kurtz Brothers	Jason Ziss	Facility Manager	216-496-0905
Kurtz Brothers	Dan Barton	Facility Operations	330-719-8391
Dredging Contractor TBD	TBD	TBD	TBD

Phasing

The spring 2023 dredge cycle is anticipated to commence in early May 2023 and to last 4-6 weeks in total, depending on weather and final dredge volumes. The USACE has not bid this contract at this time, but based on historical volumes, approximately 60,000-80,000 CY of sediment will be dredged from the Cuyahoga River and deposited into the Facility from marine-based equipment staged on the north side of CDF 9 with limited upland support from land-based earthwork equipment and piping infrastructure. This work is completed by the USACE's dredging contractor through authorization granted by the Cleveland-Cuyahoga County Port Authority. Limited inbound/outbound Facility truck and vehicle traffic supporting this operation should be anticipated for equipment deliveries, supplies/materials, employee access, maintenance technicians, inspectors, USACE personnel, etc. Note that additional dredge material might be deposited into USACE's CDF 10B, which is outside the scope of this CSPP.

The fall 2023 dredge cycle is anticipated to commence in October/November of 2023 and is anticipated to last 4-6 weeks in total. The USACE has not bid this contract at this time, but based on historical volumes, approximately 60,000-70,000 CY of sediment will be dredged from the Cuyahoga River and deposited into the Facility from marine-based equipment staged on the north side of CDF 9 with limited upland support from land-based earthwork equipment and piping infrastructure. This work is completed by the USACE's dredging contractor through authorization granted by the Cleveland-Cuyahoga County Port Authority. Limited inbound/outbound Facility truck and vehicle traffic supporting this operation should be

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anticipated for equipment deliveries, supplies/materials, employee access, maintenance technicians, inspectors, USACE personnel, etc.

The spring 2024 dredge cycle is anticipated to commence in early May 2024 and to last 6-8 weeks in total, depending on weather and final dredge volumes. The USACE has not bid this contract at this time, but based on historical volumes and remaining on-site capacity, approximately 120,000 CY can be anticipated to be dredged from the Cuyahoga River and deposited into the Facility from marine-based equipment staged on the north side of CDFs 9 & 12. Upland support will be provided from land-based earthwork equipment and piping infrastructure. Material in this cycle will be deposited into both CDFs and will consist of a hydraulic pumping and placement operation (inbound cover photo) and a mechanical offloading cycle that will include the use of excavators and material handling/transport equipment internally within the site on CDF 12. This work is completed by the USACE's dredging contractor through authorization granted by the Cleveland-Cuyahoga County Port Authority. Limited inbound/outbound Facility truck and vehicle traffic supporting this operation should be anticipated for equipment deliveries, supplies/materials, employee access, maintenance technicians, inspectors, USACE personnel, etc. Note that material will only be placed into CDF 12 if the Port Authority gains FAA permission to construct new berms to receive the material (separate, standalone CSPP and 7460 to be submitted in fall of 2022 with construction beginning in the spring of 2023).

The fall 2024 dredge cycle is anticipated to commence in October/November of 2024 and is anticipated to last 4-6 weeks in total. The USACE has not bid this contract at this time, but based on historical volumes, approximately 60,000-80,000 CY of sediment will be dredged from the Cuyahoga River and deposited into the Facility from marine-based equipment staged on the north side of CDF 9 with limited upland support from land-based earthwork equipment and piping infrastructure. This work is completed by the USACE's dredging contractor through authorization granted by the Cleveland-Cuyahoga County Port Authority. Limited inbound/outbound Facility truck and vehicle traffic supporting this operation should be anticipated for equipment deliveries, supplies/materials, employee access, maintenance technicians, inspectors, USACE personnel, etc.

The outbound movement of dredge sediment from the facility occurs year-round and is necessary to continuously regenerate the facility's appx. 160,000 CY (80,000 CY/Dredge Cycle) of annual hydraulic processing throughput capacity. When dredge sediment is hydraulically delivered to the site by the USACE's dredging contractor, it is dewatered, excavated, and stockpiled on site by the Port Authority's site operator, Kurtz Bros. This material is then loaded into dump trucks as part of the CSPP and removed from the Facility. This operation moves approximately 160,000 CY of the 250,000 CY (+/-) of dredge sediment received annually from the Facility. Once dewatered and dried, the site operator also performs a limited amount of augmenting, screening, and blending of dredge sediment with imported organic compost and other material prior to it leaving the site.

The movement of trucks into and out of the Facility to support this operation is the backbone of the operation and critical to providing capacity. To assist in understanding the scale and the need of the operations to respond to external forces, the Cleveland-Cuyahoga County Port Authority has added supplemental information in this section of the CSPP. It is being provided to assist stakeholders in understanding the scale of operation and vehicle movements associated with the successful operation of the Facility. The movement of 160,000 CY (+/-) of dredge sediment from the facility accounts for appx. 9,000 annual dump truck movements through the Facility. If averaged over the entire operating calendar of the year, this would equate to appx. 36 dump truck movements into and out of the facility per day (9,000 truck movements/250 days) specifically tied to moving sediment off site. This rate does not account for the annual soil and construction cycles in NE Ohio that traditionally slowdown/stop in the winter months and peak in the spring and summer. This market drives beneficial reuse demand from this Facility. The 36 inbound and outbound dump truck movements do not account for access associated with deliveries of material to perform site maintenance and dredge sediment material augmenting, dredging contractor material and equipment deliveries, maintenance technicians, fuel deliveries, sanitation services, supply delivery, employees, inspectors, government officials, traffic associated with other CSPPs for capital improvement projects, and contractor's staff/management. Outbound beneficial reuse movements are also impacted by the internal resources on site required to support the inbound placement and extraction of dredge sediment related to the annual deposition cycles, dewatering/drying, weather, site and equipment maintenance, BKL Airshow shut downs, variability of soil and moisture contents which limits the amount of material that can be loaded into trucks, variances in the inbound placement needs of the Federal government, dump truck cycle times between the Facility and the end distribution point, etc. As a result, there are days where traffic is limited to the site employees and staff that working at the Facility and there are peak activity days where traffic counts of 40+ vehicle movements into and out of the Facility per hour could be expected.

Appendix B includes diagrams outlining activities that will occur on the CDFs related to the movement of dredge sediment through the Facility including traffic haul routes, signage plans, equipment storage locations and details related to the airport access route and the controlled access point located at the SE corner of Burke Lakefront Airport. Appendix C includes work points and elevations tied to activity at the Facility.

Areas & Operations Affected by Activity

The area of the Airport most greatly impacted by the Port Authority's operations will be the access road used to gain access to the CDFs by the Contractor(s). This roadway was originally constructed in the late 1960's as part of the construction of the CDFs north of the Airport to provide and preserve access to and from the CDFs. This access roadway was realigned in 2013 as part of the 600' 6L/24R runway extension. Note, BKL has imposed a modification to the declared distances that has shortened the OFA by a distance great enough to remove the access haul route/VSR from the OFA. The RSA butts up against the access roadway but is not known to cross the access roadway. The access road and a portion of CDF 12 fall in the Runway 6L/24R Approach and Departure RPZs. See the diagrams in Appendix B for reference.

Mitigation efforts to clearly and safely direct contractors, personnel, and truck traffic along the access roadway and prevent vehicles from entering the safety areas are clearly detailed below and further detailed in the diagrams provided in Appendix B. Additionally, for scheduled air carrier operations, the Port Authority and Contractor(s) who support of the Port Authority's operations will be required to vacate the main access roadway leading to and from the CDFs. BKL's operation's desk will be responsible for providing timely notice to the Port Authority's Site Security and Access Control Team in order to vacate and restrict access to and from the CDFs. Access will be restricted on site by the Port Authority's full time Site Security & Access Control staff.

Protection of Navigation Aids (NAVAIDs)

Contractors on site will be responsible for controlling fugitive dust in accordance with existing OEPA and City of Cleveland air permits. This includes keeping both the haul roads and stockpiles near their optimum moisture contents to limit dust to the greatest extent possible.

In the event BKL's Automated Surface Observing System (ASOS), Medium Intensity Approach Lighting System (MALSR), or any other system becomes impacted as a result of the Port Authority's operation, immediate and corrective measures will be taken by the Port Authority, Airport, and/or our Contractors. If the Port Authority's operations on the CDFs are found to be the root cause of the issue, operations on the CDFs will stop until corrective measures can be implemented to protect BKL's NAVAIDs.

No activity that will knowingly impair the reporting capability of any airfield equipment will be permitted to take place.

Contractor Access

The Port Authority, in partnership with Cleveland Airport Systems (CAS), the Port Authority's Site Security & Access Control Team (Allied Universal), and our Contractors will implement the following access control program at the Facility to ensure compliance with requirements of 14 CFR Part 139. The program will limit access and unescorted driving privileges to those individuals who receive driver safety training from Burke Lakefront Airport. All visitors who do not have this training will be required to be escorted while on site. Each Contractor and the Port Authority's Site Security & Access Control Team will be responsible directly and financially for all vehicles and personnel directly related to their scope of work. Contractors will be required to work with the Port Authority's Site Security & Access Control Team to comply with the requirements of this CSPP. This includes all employees, vendors, inspectors, and delivery drivers that visit the site. Contractors shall designate a site safety and security supervisor for their operations to ensure compliance with this CSPP and to coordinate directly with the Port Authority's Site Security & Access Control Team. Nicholas LaPointe and Carly Beck will serve as the Port Authority's Facility Managers and Site Safety & Security Supervisors.

An airfield driver safety training course will be offered by appointment at BKL by a CAS employee and by special appointment as needed. Joel Woods (216-664-4530) with Burke Lakefront Airport will be contacted to schedule and coordinate this training. This training is

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specific to Burke Lakefront Airport with specific information related to work on the CDFs. All contracted employees working at the Facility or those who routinely visit the Facility, including regular service providers and dump truck drivers, will be required to attend this training and obtain their access credentials (code, lanyard, & card) in advance of coming into the Facility.

Members of the Port Authority's Site Security & Access Control Team, who will serve as site escorts and will control and monitor access in and out of the Facility during normal business hours, will be required to obtain additional training over and above the standard access and driver safety training course offered by BKL. CAS will assist the Port Authority in scheduling the expanded training for our Site Security and Access Control Team. CAS will be responsible to notify the Port Authority a minimum of 30 days in advance of personnel that need to refresh their site access and driver safety training on an as needed basis determined by CAS.

Following the successful completion of this training, each trainee will receive a unique identifier access code/number, a lanyard, and an identification card provided by CAS. The access code will be unique to the individual and will be kept in a database maintained and managed by CAS. The access code will permit access through the controlled access point at the east end of the Airport and will permit unescorted access to and from the CDFs, 24-7. Personnel that have received their access control credentials and driver safety training will enter their unique identifier access code and Facility access will be authorized. This system will automatically log personnel as they enter the Facility for record keeping purposes. In addition, these people will be expected to have their lanyards and ID cards on them at all times for verification once inside the Facility.

Guests and visitors without driver safety training and access credentials, will have the ability to directly page the Port Authority's Site Security & Access Control Team housed in the office trailer immediately adjacent to the controlled access point during normal business hours (7am-4pm M-F). The Port Authority's Site Security & Access Control Team Leader will maintain an accurate record of all visitors and guests associated with our operations that are granted access through the gate related to our operations requiring escorts. It will be the responsibility of the Contractor(s) in partnership with the Port Authority's Site Security & Access Control Team to ensure all visitors have an appointment and an intended business purpose on site prior to their arrival. Any visitor that is unable to be properly vetted by the Port Authority's Site Security & Access Control Team or Contractor shall be turned away and not permitted access onto Airport property. The Port Authority's Site Security and Access Control Team Leader is responsible for granting a visitor access. ACCESS SHALL NOT BE GRANTED UNTIL THE VISITOR & BUSINESS PURPOSE HAS BEEN PROPERLY VETTED.

During normal business hours (7am-4pm), the Port Authority will staff 2 EA. full time employees as part of the Site Security & Access Control Team. Note, depending on facility needs and time of day, the Port Authority Site Security & Access Control Team staffing levels may vary. The team leader will be stationed full time at the front gate and will be responsible for vetting and logging in visitors/guests requesting Facility access. Personnel who obtain access using their unique identifier code will be automatically logged in the Airport's system and will not be

screened or escorted by the Port Authority's Site Security & Access Control team. Guests and visitors without training will be required to be signed in by the Port Authority's Site Security & Access Control Team. The other member of the team will serve as a full-time escort during normal business hours. Escorts will be provided on a first come, first serve basis. During peak times of outbound sediment movement from the Facility, Contractors may be required to supplement the Port Authority's full time escorting staff in order to maintain the required escort ratios or trucks will be forced to wait in queue at the entrance of the facility for an available escort. Contractors working on site shall schedule all of their deliveries and the outbound movement of dredge sediment during delivery hours from 7am to 3pm, M-F. Contractors working outside or beyond these standard hours will be responsible for performing their own site access control and escorting outside of normally staffed business hours (7am to 4pm). Contractor(s) shall follow this detailed policy as if they were acting as the Port Authority Site Security and Access Control Team.

During peak times of the year, when the keypad system may create access and egress bottleneck in and out of the Facility, the Port Authority shall have the ability to resort to traditional I.D. badge verification, manual operation of the gate, and the verification and the logging in of all personnel at the access control gate to expedite and efficiently move vehicles into and out of the Facility. When this gate system override is in place, the gate movements will be controlled by the Port Authority's Site Security & Access Control Team as was done prior to the keypad system being installed in 2019. Guests/visitors with proper lanyards and ID cards will be permitted access and guests/visitors that do not have the proper access credentials will be escorted. This manual screening method will only be done if and when the Port Authority's Site Security & Access Control Team is fully staffed and in place and on limited days when Facility demands warrant it in close coordination with the Airport.

All untrained visitors/guests will be met at the front gate by an available escort after being signed into the facility by the Site Security & Access Control Team Leader. The escort will be responsible for briefly discussing safety protocols and the escort policy to new visitors to the Facility. The escort policy for this Facility will be built on maintaining clear line of sight, a direct line of communication, and maintaining of close proximity to all visitors and guests. Each person that has received the access and driver safety training at BKL shall have the ability to escort as many as five (5) visitors/guest at any time in up to three separate vehicles. For special events, particularly tours of the facility to local stakeholders, government officials, and other V.I.P.s, the ratio can be increased to 10:1. However, the max number of vehicles shall remain within the 3:1 ratio. Special tours that require temporarily increasing the escorting ratio shall be done in coordination with Burke Lakefront Airport personnel. The foundation of the escort policy is detailed below and shall be carried out as follows:

- Clear Line of Sight:
 - All visitors whom have not received the Site Access and Driver Safety Training from BKL must be logged in and escorted while on site. This includes vehicles in transit to and from the CDFs and while on the CDFs.

Escorts must maintain a clear line of site to the visitor at all times and at no time shall an escort be responsible for more than 3 vehicles at one time.

- Escorts shall have the ability to transfer visitors/guests between them so long as they operate within their escort ratios, however, at no time shall escorts be transferred to the equipment operators who are performing other duties/functions on site. The only exception/unique scenario shall be if the Port Authority's Site Security & Access Control Team has reached the end of their shift and the Contractor provides relief by supplying their own dedicated and qualified escort personnel to serve as a designated facility escort and provide coverage.
- A copy of the sign in logs will be provided to the Airport and to the Port Authority on a daily basis or as frequently as requested. Repeat drivers to the facility that has not received access and driver safety training, will be flagged and requested to obtain training to continue to performing business on site by the Port Authority.
- Direct Line of Communication:
 - Visitors to the facility that are equipped with CB radios will be directed to monitor a specific radio frequency to maintain direct communications with their facility escort at all times. If not equipped, the escort will be required to establish an alternative means of communication via cell phone or via providing a loaner radio to the visitor for use while on site.
 - At all times, escorts shall remain in direct contact with the Site Security & Access Control Team leader stationed in the trailer near the access control gate at the entrance to the facility. The Team Leader stationed at the entrance to the facility will remain in contact with the site escorts, all contractors working on site, BKL's operations desk, and routinely report back to the Port Authority's safety command center located at the Port of Cleveland. This person will be responsible for coordinating the escorts, notifying Contractors they have visitors, and logging the in and out movement of all personnel through the Facility. Contractors shall provide the Site Security & Access Control Team leader daily notices of any scheduled or special visitors at the start of each day.
- Close Proximity:
 - The escorts must maintain "close proximity" to all employees and visitors that have not received the site access and driver safety training provide by CAS. Close proximity, means the escort shall remain situationally aware of the position of this visitor at all times while on the CDFs and shall have the ability to intercept or perform corrective measures in the event the visitor/guest travels outside of an authorized areas of operation and into an active safety area of the Airport. The corrective measures may involve physical intercepting of the path of travel or verbal direction

via a direct line of communication. Escorts shall have the ability to immediately notify the Airport of the actions of a visitor which may present a hazard to the Airport. Depending on the business purpose of the guest/visitor, the definition of close proximity may vary from one individual to the next. For example, a surveyor gathering field data on foot may vary from a truck driver getting loaded out with dredge sediment.

Vehicles transiting along the Airport Perimeter Roadway and Facility haul routes shall not exceed 14'-10" in overall height at any time unless specially coordinated with the Airport in advance. In 2018, under the guidance of CAS, signage was placed along the main airport access haul route directing vehicles transiting along the airport access roadway to fully clear the airport access roadway and proceed to the CDFs, no stopping or idling shall be permitted along the airport access roadway. Additional signage was placed near the entrance to the Facility, just beyond the Airport's maintenance facility, restricting access into this area of the airport. The Port Authority also installed signage at the entrance of the Facility with additional site access and operational information.

As previously indicated in this CSPP, through coordination with BKL's operations desk, the Port Authority's Site Security & Access Control Team will coordinate the clearing of the OFA and the main access road adjacent to the RSA when scheduled air carrier flights are approaching or departing the Airport. Note, BKL has imposed a modification to the declared distances that has shortened the ROFA by a distance great enough to remove the access haul route/VSR from the ROFA. With this modification, no activity should take place within the ROFA.

STOCKPILES

Stockpiling of dredge sediment will be required to support our 2023-2024 operations and the locations of these stockpiles have been included in diagrams in work point and working height elevations of Appendix C. Note, many of the stockpile dredge sediment management areas are located within the RPZ that extends onto CDF 12.

The temporary stockpiles of dredge sediment will be limited to a max height of 15' or a max elevation +606, whichever comes first. Material stockpiled and staged is only temporarily staged in the designated areas until it is loaded out in a truck and removed from the site. In order to facilitate load out operations, the equipment performing the load out will exceed the height of the stockpiles, specifically as the bucket of the excavator works the top of each of the stockpiles and places it into the dump trucks. This equipment shall have a permitted maximum working height of 20' on site unless noted otherwise in specific work areas. At no time will equipment be permitted to sit on top of the stockpiles.

VEHICLES, EQUIPMENT PARKING. & FUEL TANK STORAGE

Contractors shall limit bringing personal vehicles onto airport property, beyond the secure access control check point. Limited parking accommodations for personal vehicles shall be permitted near the front gate access control point and site security and access control office.

Parking inside the secure limits of the Airport is limited along the east side the maintenance facility and in front of the Facility's site trailer. At no time, shall personal vehicles transit along the airport access roadway and out onto the CDFs. Contractors shall keep a log of personal vehicles on site, along with a list of employee license plate numbers and make/models of vehicles. Parking passes shall be displayed in the front window of these vehicles linking the vehicles to a specific Contractor working on site along with the direct contact information for the vehicle's owner in the event they need to be relocated or removed.

Equipment will be stored and staged in the locations included in the attached Construction Phasing Diagrams in Appendix B. At night all equipment shall be stowed in the lowest elevation configuration. At no time will the Port Authority or our Contractors store equipment inside the OFA or RSA. Contractor shall limit the parking and placement of equipment to the greatest extent possible within the soil stockpile area on CDF 12. Limited mobile equipment that could include soil screening or earth handling equipment may be placed and staged with the soil stockpile area of CDF 12. The Port Authority and our Contractors will not store any fuel tanks unless authorized by a separate permit, install any misleading lighting, or perform restricted operations within the RPZ that will create glare or attract wildlife. If the Contractor seeks to bring in temporary fuel tanks to assist with their operations, tanks will be positioned outside the RPZ and proper permits and airspace determination requests will be filed and obtained with the FAA and City of Cleveland under separate cover.

Trucks that come into the facility to be loaded out with dredge sediment, will be loaded immediately upon their arrival and then will be directed to exit the Facility. Contractor shall route and direct traffic within the Facility in such a manner which keeps it from backing up along the APR if and when trucks are waiting to be loaded out with beneficial reuse sediment.

EQUIPMENT MARKING

Construction equipment operating on the CDFs shall display in full view above the vehicle a 3'X3' or larger, orange and white checkerboard flag, each checkerboard color being 1' square. Construction vehicles and equipment shall be marked with a sign on both the driver and passenger side of the vehicle identifying the contractor's name. As these flags age and become less visible/dingy, Contractors shall replace the flags.

During periods of low visibility, as determined by BKL Operations or BKL Control Tower, all equipment and trucks operating will be required to have rotating beacons. Beacons will only be in use when requested by BKL Operations or the BKL Control Tower.

ACCESS HAUL ROUTES & ESCORTS

Access haul routes are designated in Appendix B. The access haul routes will be marked with signage to prevent inadvertent entry into active airport areas. At no time shall any of equipment impede or limit any aircraft rescue and firefighting on airport property. At no time will airport operations or wildlife management be impeded by equipment or the Port Authority's operations

on the haul routes. For information related to escorting, please see the details included in the Contractor Access section above.

Wildlife Management

The Port Authority participates with Burke Lakefront Airport's wildlife management plan. The Port Authority is one of three parties that participates and have cooperation agreements to financially support the USDA with wildlife management services for Burke Lakefront Airport. These services occur both on the airport and on the CDFs. The Port Authority's presence and more regular maintenance on the site will continue to deter wildlife from the areas we occupy and regularly turn over. Any wildlife that is seen on site, outside of the common passing waterfowl or bird that may cause a hazard to Airport operations will be passed along to the USDA staff.

TRASH

Any and all trash generated from this operation that may attract wildlife (food and organic material) will be bagged and removed daily from the Facility by contractor(s). If there are activities on site that will generate inorganic construction debris, covered dumpsters will be used with signage permitting only construction debris permitted. These dumpsters will be emptied on a weekly basis or as needed to ensure they remain covered and fully closed at all times.

STANDING WATER

Through active management of the Facility, the Port Authority will move all water associated with the receipt and processing of the dredge sediment from the Facility as soon as possible in compliance with permits with the Ohio EPA, during and following the deposition of the dredge sediment. Passive stormwater systems have been installed at the Facility to actively manage and move stormwater from the surfaces of the Facility to reduce the presence of water. Note, this does not include CDF 10B.

TALL GRASS & SEEDS

The Port Authority will manage vegetation on our activated portions of the CDFs as needed. This may include periodic mowing and cutting of vegetation in inactive areas of our operation and on the exterior of berms.

FOREIGN OBJECT DEBRIS MANAGEMENT (FOD)

Contractors on site will be responsible for controlling fugitive dust. This includes keeping both the haul roads and stockpiles at their optimum moisture contents to limit dust and to keep roadways free of rutting and excessive silt build up. There are no construction materials associated with this Project.

All complaints received as result of road debris and/or fugitive dust will be received and responded to by the Port Authority and/or the Port Authority's Contractors. Port Authority contractors will be responsible for maintaining a log of complaints received.

HAZARDOUS MATERIALS MANAGEMENT (HAZMAT)

Permitted fueling operations for equipment will occur on the CDFs. Presently, all equipment is fueled using a fuel truck that makes regular deliveries to CDFs 9 & 12. This fuel truck is equipped with spill prevention and containment equipment. Fueling of equipment occurs outside of the RPZ on CDFs 9 & 12. In the future, Contractors may elect to bring temporary fuel storage tanks on site. Tanks will be set up with secondary containment measures, barriers, and equipped with spill containment kits. Prior to bringing a fuel tank onto CDF 12, spill response plans must be on file with the Port Authority, permits will be needed from the City of Cleveland, and a separate airspace analysis must be performed by the FAA through the filing of a standalone 7460 submission. At no time will any fuel tanks be placed within the RPZ limits that extend onto CDF 12.

The Contractor shall store all hazardous materials in containers approved for such use, shall have the Material Safety Data Sheets (MSDS) on site for all such materials, and shall have cleanup materials recommended by the MSDS on site and readily available for use in the event of a spill. All materials shall be used in strict accordance to the manufacturer's instructions and should be prepared to meet inspection at any time.

Transfer of materials from one container to another shall be done in areas where a spill is least likely to cause damage such as away from streams, storm sewer inlets, etc. Fueling of vehicles and equipment shall be done at designated areas within the staging areas shown on the attached Phasing Plans. Appropriate spill kits shall be available for all refueling operations. All contaminated materials from used spill kits shall be removed from the site and properly disposed of immediately after use.

Any spill, regardless of size, must be reported to the Port Authority & Airport.

The following instructions are taken from AC 150/5320-15A, "*Management of Airport Industrial Waste*" and the Airport's spill management plan should be followed in the event of a hazardous spill:

Cleanup-General

Clean up leaks and spills immediately. Use a rag, absorbent pad, or other suitable material for small spills on paved surfaces, and absorbent material for larger spills. If the spilled material is hazardous, then the used cleanup materials are also hazardous and must be sent to either a certified laundry (rags) or disposed of as hazardous waste in designated areas.

Never hose down or bury dry material spills. Clean up as much of the material as possible and properly dispose of legally off the Facility and airport property.

Minor Spills

Minor spills typically involve small quantities of oil, gasoline, paint, etc. which can be controlled by the first responder at the discovery of the spill.

- Contain the spread of the spill and prevent it from entering any of the existing retention ponds or Lake Erie.
- Notify the project foreman immediately
- Use absorbent materials on small spills - DO NOT hose down or bury the spill. Recover spilled materials.
- Absorbent materials should be promptly removed and properly disposed of legally off airport property.
- Clean the contaminated area and properly dispose of contaminated materials legally off airport property

Semi-Significant Spills

Semi-significant spills still can be controlled by the first responder along with the aid of other personnel such as laborers and the foreman, etc. This response may require the cessation of all other activities. The Contractor shall immediately notify Office of Airport Operations at 216-781-6411.

- Contain the spill and prevent it from entering any of the existing retention ponds or Lake Erie
- Notify the project foreman immediately.
- If the spill occurs on paved or impermeable surfaces, clean it up using “dry” methods (absorbent materials and do not let the spill spread widely)
- If the spill occurs in dirt areas, immediately contain the spill by constructing an earthen dike. Dig up and properly dispose of contaminated soils.
- If the spill occurs during rain, cover the spill with tarps or other material to prevent contaminating runoff.

Significant/ Hazardous Spills

For significant or hazardous spills that cannot be controlled by personnel in the immediate vicinity, the following steps should be taken:

- The Contractor shall immediately notify Airport Operations at 216-781-6411.
- The Airport will notify the proper officials as per the spill plan.

- The construction personnel should not attempt to clean up the spill until the appropriate and qualified staffs have arrived at the spill site.
- The Contractor shall complete the airport's spill form written report.

The services of a spill contractor or a HAZMAT team should be obtained immediately, and such response will be coordinated between the Airport and the Contractor.

Reporting

The Contractor must report significant spills to the Airport and shall comply with all Airport reporting requirements.

Notification of Construction Activities

The Port Authority will notify Anthony Campofredano & Joel Woods at Burke Lakefront Airport two weeks prior to commencing inbound sediment placement activities into CDFs 9 & 12. A courtesy notification will also be sent 72 hours prior to a change in operations/activity that may be occurring at the Facility.

Inspection Requirements

This project is subject to inspections by the FAA, Airport, Engineer, ODOT Bureau of Aviation, Port Authority, Ohio Environmental Protection Agency, Ohio Division of Natural Resources, the Coast Guard, and the Army Corp. of Engineers. Contractors will be required to inspect their operations daily to ensure they are operating in compliance with the CSPP and their SPCD. If Contractor's have special operations that will require separate 7460 filings, it will be the responsibility of the Contractor to notify the Port Authority to coordinate the filing of these cases with the Port Authority, Airport, and FAA in a timely manner to permit the FAA sufficient time to perform their review and issue a determination. The FAA requires a minimum of 45-60 days prior to special operations occurring that will require an additional 7460 airspace filing and review.

Underground Utilities

There are no known active utilities in the Project area.

Penalties

If any contractor, vendor, or subcontractor working on behalf of the Port Authority at the CDFs, is found to be non-compliant with Airport rules and regulations, that specific person will be removed from the Facility and prevented access in the future. If a specific contractor, vendor, or subcontractor violates any of the airport rules and regulations, the Port Authority or the Airport reserves the right to restrict them from doing business at the CDFs. Federal penalties that may result out of failure of a contractor to comply with this CSPP and other Federal safety/security requirements and shall be the full responsibility of the contractor.

Special Conditions

The Port Authority and our contractors understand that at certain times, specialized short term access and operations restrictions may be put in place as result of special aircraft using Burke Lakefront Airport and the annual Cleveland Air Show held at the Airport. The Port Authority will be notified of any such events.

Runway & Taxiway Visual Aids

Contractors on site will be responsible for controlling fugitive dust and keeping both the haul roads and stockpiles at optimum moisture content to limit dust as best possible.

In the event that BKL's Automated Surface Observing System (ASOS), Medium Intensity Approach Lighting System (MALSR), or any other system, become impacted as a result of this operation, immediate and corrective measures will be taken by the Port Authority, Airport, and/or Contractor(s). No activity that will knowingly impair the reporting capability of any airfield equipment will be permitted to take place.

Marking & Signs for Access Route

Haul Routes - The haul route to the project site is off an existing public road (North Marginal Rd.) as shown in Appendix B. All truck access inbound and outbound shall come from or exit the Facility towards East 55th Street. Public and Airport access roads leading to the project site shall be used as haul routes and shall be maintained by the Contractor(s). All public right of way haul routes shall be kept free of all mud and debris. Contractor shall sweep roads when necessary and when directed by Airport or Port Authority personnel. The condition of the main access road to and from the CDFs which also services as the Airport's Perimeter Road will be monitored and maintained in good working order by contractors working on site. Access roads out on the CDFs, outside of the Airport Perimeter Road, shall not be used or accessed by the Airport personnel unless specially coordinated by the Port Authority. As result of the nature of our activity, some of these roads experience condition issues certain time of the year that make transiting of some of these roads difficult for some vehicles. All haul routes across the airport and on the CDFs are highlighted in the Appendix B documentation.

Signage was installed along the main access haul route in 2018 directing traffic to and from the CDFs and limiting traffic from entering active airport areas. CAS provided the Port Authority guidance on the specific signage requested along with specifications and the Port Authority installed these signs.

Hazard Marking & Lighting

The work under this Project shall comply with the FAAs lighting & marking guidelines and/or additional requirements in the FAAs airspace determination.

Work Zone Lighting for Nighttime Construction

Work under this CSPP is planned to be performed during daylight hours. Operations may need to be adjusted at certain times of the year to maximize daylight and limit operations in low/restricted visibility, no artificial lighting is planned to be used.

Protection of Runway and Taxiway Safety Areas

(a) Runway Safety Area (RSA)

There shall be no work within or transiting of any active Runway Safety Area (RSA). The Contractor is not permitted to work within the RSA of an active runway. Prior to commencement of any work adjacent to an active runway, the contractor shall delineate the RSA and shall not commence work in that area until the work area is clearly delineated from the RSA.

(b) Runway Object Free Area (ROFA)

BKL has imposed a modification to the declared distances that has shortened the ROFA by a distance great enough to remove the haul route/VSR from the ROFA in its entirety. With this modification, no activity shall take place within the ROFA.

(c) Taxiway Safety Area (TSA)

The contractor shall not be permitted to work within an active TSA at any time.

(d) Taxiway Object Free Area (TOFA)

The contractor shall not be permitted to work within an active TOFA at any time.

(e) Runway Obstacle Free Zone (OFZ)

There will be no work within any active Obstacle Free Zone (OFZ).

(f) Runway Protection Zone (RPZ)

No permanent, fixed structures are proposed inside the area of the RPZ that extends onto active dredge sediment handling areas of CDF 12. Stockpiling, processing, and material load out operations are proposed to occur inside the RPZ. When possible, unoccupied tracked mobile equipment will be removed from the RPZ on a nightly basis, excluding soil screening equipment.

(g) Runway Approach/Departure Areas and Clearways

Work within any Runway Approach/Departure Areas or Clearways will be evaluated by the FAA. No work shall be permitted within the primary surface.

Other Limitations on Construction

- As result of the complexities and need for this Facility to be able to adapt to external forces and conditions, so long as the provisions of this CSPP are being followed, there

2023-2024 CONSTRUCTION SAFETY & PHASING PLAN: ANNUAL SITE OPERATIONS

is no limit on the number of hourly vehicles that may gain access to the Facility.

- Smoking is not permitted anywhere on the AOA at any time, however, it is permitted on the CDFs in areas outside of the AOA.
- Hot work permits must be obtained from ARFF/Cleveland Fire for any work involving an open flame.
- The Contractor shall not use equipment that is over 15' feet in height as indicated on the Safety and Phasing plans unless clearly identified in 7460 equipment filings.
- This CSPP covers the Cleveland-Cuyahoga County Port Authority's 2023-2024 planned site operations related to the inbound placement of dredge sediment into the Port Sediment Processing & Management Facility and the outbound movement of approximately 60% of this material from the facility via outbound over the road truck movements.
- Landside beneficial reuse site contractor working hours will be 7:00 A.M. to 4:00 P.M. daily, with loadout operations occurring from 7:00 A.M. to 3:00 P.M. daily (M-F).
- Dredging contractor working hours shall be 24-7 during inbound dredge placement into the facility (hydraulic/mechanical) that occurs twice annually.

Attachments

- APPENDIX A: SPCD COMPLIANCE FORM
- APPENDIX B: CONSTRUCTION PHASING DIAGRAMS
- APPENDIX C: WORK POINT & ELEVATION TABLES

APPENDIX A

SAFETY PLAN COMPLIANCE DOCUMENT
for
CITY OF CLEVELAND, DEPARTMENT OF PORT
CONTROL, & PORT OF CLEVELAND
Burke Lakefront General Aviation Airport
Sediment Processing & Management Facility

“I, _____ (NAME), authorized representative of _____ (CONTRACTOR), have read the “Construction Safety and Phasing Plan for City of Cleveland, Department of Port Control, Sediment Processing Facility and will abide by it as written and with the following additions as noted:

Coordination

- (a) Contractor Progress Meetings
- (b) Scope or Schedule Changes
- (c) FAA ATO Coordination

Phasing

- (a) Phase Elements
- (b) Construction Phasing Drawings

Areas and Operations Affected by Construction

- (a) Identification of Affected Areas
- (b) Mitigation of Effects

Navigational Aid (NAVAID) Protection

Contractor Access

- (a) Location of Stock Piled Materials
- (b) Vehicle and Pedestrian Operations

Wildlife Management

- (a) Trash
- (b) Standing Water
- (c) Tall Grass and Seeds
- (d) Poorly Maintained Fencing and Gates
- (e) Disruption of Existing Wildlife Habitat

Foreign Object Debris (FOD) Management

Hazardous Materials (HAZMAT) Management

- (a) Cleanup-General
- (b) Minor Spills
- (c) Semi-Significant Spills
- (d) Significant/Hazardous Spills
- (e) Reporting

Notification of Construction Activities

-
- (a) List of Responsible Representatives
 - (b) Notices to Airmen (NOTAM)
 - (c) Emergency Notification Procedures
 - (d) Coordination with ARFF Personnel
 - (e) Notification to the FAA

Inspection Requirements

- (a) Daily Inspections
- (b) Final Inspections

Underground Utilities

Penalties

Special conditions

Runway and Taxiway Visual Aids, Marking, Lighting, Sign and Visual NAVAIDS

- (a) General
- (b) Markings
- (c) Signs

Marking and Signs for Access Routes

Hazard Marking and Lighting

- (a) Purpose
- (b) Equipment
- (c) Personal Safety

Protection-Runway and Taxiway Safety Areas, Object Free Areas Obstacle Free Zone and Runway Approach/Departure Surfaces

- (a) Runway Safety Area (RSA)
- (b) Runway Object Free Area (ROFA)
- (c) Taxiway Safety Area (TSA)
- (d) Taxiway Object Free Area (TOFA)
- (e) Runway Obstacle Free Zone (OFZ)
- (f) Runway Approach / Departure Areas and Clearways

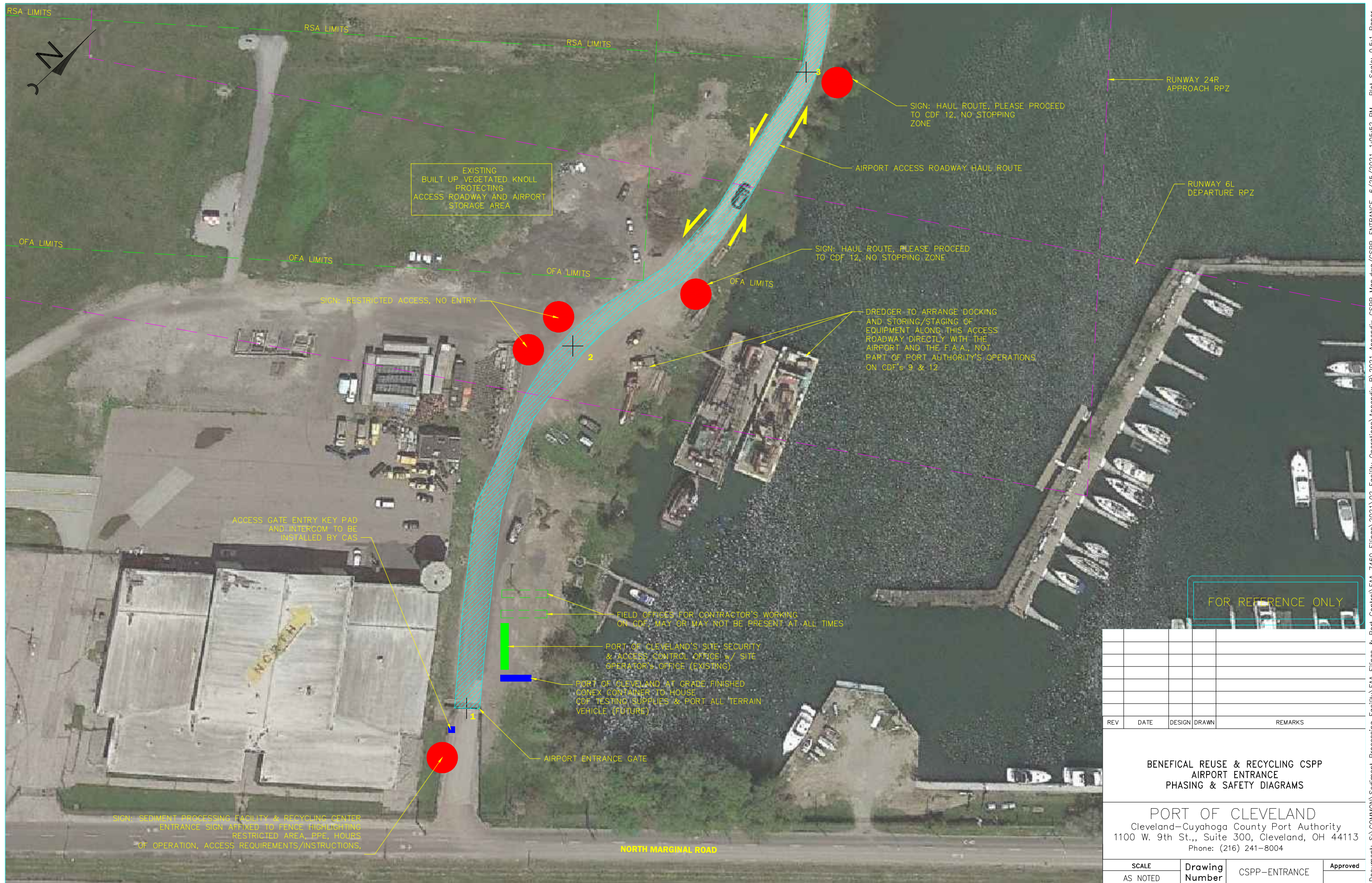
Other Limitations on Construction

- (a) Prohibitions
- (b) Restrictions

Included Appendices – Safety and Phasing Plan

Signature: _____ Date: _____

APPENDIX B



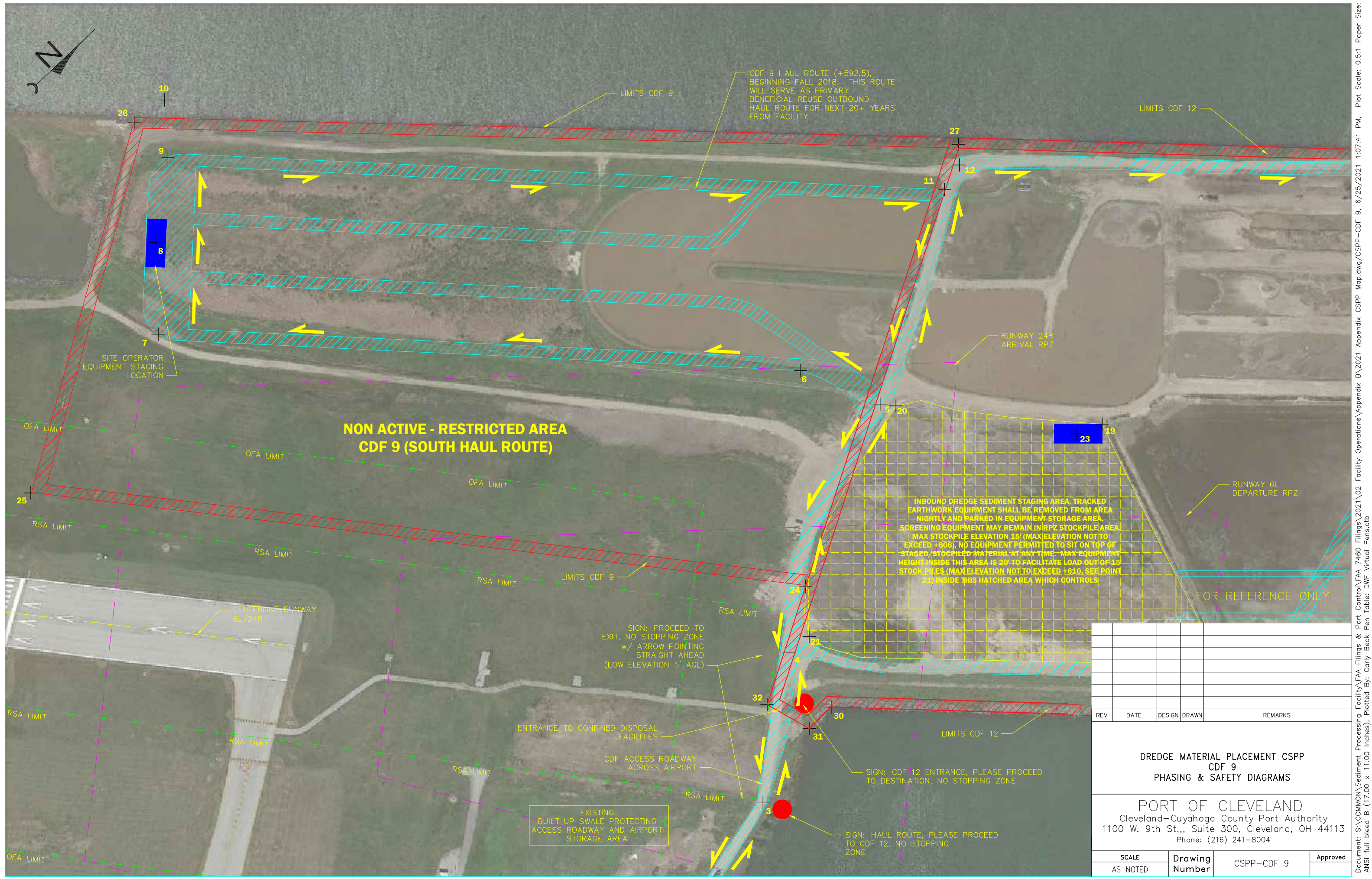
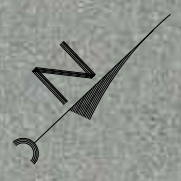
REV	DATE	DESIGN	DRAWN	REMARKS

**BENEFICIAL REUSE & RECYCLING CSPP
AIRPORT ENTRANCE
PHASING & SAFETY DIAGRAMS**

PORT OF CLEVELAND
Cleveland-Cuyahoga County Port Authority
1100 W. 9th St., Suite 300, Cleveland, OH 44113
Phone: (216) 241-8004

SCALE AS NOTED	Drawing Number CSPP-ENTRANCE	Approved	
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Document: S:\COMMON\Sediment Processing Facility\FAA Filings & Port Control\FAA Filings\2021\02 Facility Operations\Appendix B\2021 Appendix CSPP Map.dwg/CSPP-ENTRANCE, 6/25/2021 1:05:52 PM, Plot Scale: 0.5:1 Paper
 Size: ANSI full bleed B (17.00 x 11.00 inches), Plotted By: Cary Beck Pen Table: DWF Virtual Pens.ctb



CDF 9 HAUL ROUTE (+592.5), BEGINNING FALL 2018. THIS ROUTE WILL SERVE AS PRIMARY BENEFICIAL REUSE OUTBOUND HAUL ROUTE FOR NEXT 20+ YEARS FROM FACILITY

**NON ACTIVE - RESTRICTED AREA
CDF 9 (SOUTH HAUL ROUTE)**

INBOUND DREDGE SEDIMENT STAGING AREA, TRACKED EARTHWORK EQUIPMENT SHALL BE REMOVED FROM AREA NIGHTLY AND PARKED IN EQUIPMENT STORAGE AREA. SCREENING EQUIPMENT MAY REMAIN IN RPZ STOCKPILE AREA. MAX STOCKPILE ELEVATION 15' (MAX ELEVATION NOT TO EXCEED +606). NO EQUIPMENT PERMITTED TO SIT ON TOP OF STAGED/STOCKPILED MATERIAL AT ANY TIME. MAX EQUIPMENT HEIGHT INSIDE THIS AREA IS 20' TO FACILITATE LOAD OUT OF 15' STOCK PILES (MAX ELEVATION NOT TO EXCEED +610. SEE POINT 23) INSIDE THIS HATCHED AREA WHICH CONTROLS

FOR REFERENCE ONLY

SIGN: PROCEED TO EXIT, NO STOPPING ZONE w/ ARROW POINTING STRAIGHT AHEAD (LOW ELEVATION 5' AGL)

SIGN: CDF 12 ENTRANCE, PLEASE PROCEED TO DESTINATION, NO STOPPING ZONE

SIGN: HAUL ROUTE, PLEASE PROCEED TO CDF 12, NO STOPPING ZONE

EXISTING BUILT UP SWALE PROTECTING ACCESS ROADWAY AND AIRPORT STORAGE AREA

REV	DATE	DESIGN	DRAWN	REMARKS

**DREDGE MATERIAL PLACEMENT CSPP
CDF 9
PHASING & SAFETY DIAGRAMS**

PORT OF CLEVELAND
Cleveland-Cuyahoga County Port Authority
1100 W. 9th St., Suite 300, Cleveland, OH 44113
Phone: (216) 241-8004

SCALE AS NOTED	Drawing Number CSPP-CDF 9	Approved
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APPENDIX C

2023-2024 CDFS 9 & 12 DREDGE SEDIMENT PLACEMENT & BENEFICAL REUSE

LOCATION	LATITUDE	LONGITUDE	ELEVATION	MAX EQUIPMENT TRANSITING HEIGHT U.N.O.	MAX ELEVATION	APPX. SAFETY ELEVATION	NOTES
1	41°31'21.83"N	81°40'3.56"W	+583'	15'	+598'	-	AIRPORT ACCESS CONTROL GATE & HAUL ROUTE
2	41°31'24.97"N	81°40'5.35"W	+583'	15'	+598'	-	AIRPORT ACCESS ROAD & HAUL ROUTE
3	41°31'29.28"N	81°40'5.18"W	+583'	15'	+598'	+610'	AIRPORT ACCESS ROAD & HAUL ROUTE
4	41°31'31.99"N	81°40'7.37"W	+591'	15'	+606'	+610'	AIRPORT ACCESS ROAD & HAUL ROUTE
5	41°31'37.32"N	81°40'9.86"W	+586'	15'	+601'	+613'	AIRPORT ACCESS ROAD & HAUL ROUTE
6	41°31'36.72"N	81°40'12.49"W	+592.5'	20'	+612.5'	+610'	CDF 9 HAUL ROUTE & EXACVATOR WORK POINT
7	41°31'28.73"N	81°40'28.30"W	+592.5'	20'	+612.5'	+612'	CDF 9 HAUL ROUTE & EXACVATOR WORK POINT
8	41°31'30.36"N	81°40'30.42"W	+592.5'	20'	+612.5'	+637'	CDF 9 EQUIPMENT STORAGE AREA
9	41°31'31.73"N	81°40'31.25"W	+592.5'	20'	+612.5'	+667'	CDF 9 HAUL ROUTE & EXACVATOR WORK POINT
10	41°31'32.86"N	81°40'32.93"W	+573'	125'	+689.0'	+692'	MARINE BASED EQUIPMENT WORK POINT (HYDRAULIC DISCHARGE)
11	41°31'45.26"N	81°39'47.86"W	+586'	20'	+606'	+672'	CDF 9 HAUL ROUTE & EXACVATOR WORK POINT
12	41°31'42.58"N	81°40'12.82"W	+582'	15'	+597'	+692'	CDF NORTH HAUL ROUTE SPLIT (TEE)
13	41°31'49.37"N	81°40'1.70"W	+583'	15'	+598'	+702'	CDF 12 HAUL ROUTE BEND TO SOUTH
14	41°31'39.30"N	81°40'5.61"W	+595'	15'	+610'	+707'	CDF 12 NE CORNER ACTIVE HAUL ROUTE
15	41°31'49.14"N	81°39'59.72"W	+595'	40'	+635'	+692'	MECHANICAL OFFLOADING EQUIPMENT STORAGE & STAGING AREA
16	41°31'50.75"N	81°40'1.65"W	+573'	125'	+689.0'	+727'	MARINE BASED EQUIPMENT WORK POINT (HYDRAULIC DISCHARGE)
17	41°31'48.47"N	81°39'58.64"W	+595'	20'	+615'	+667'	CDF 12 HAUL ROUTE & MECHANICAL OFFLOAD WORKPOINT
18	41°31'38.23"N	81°39'55.85"W	+590'	20'	+610'	+629'	CDF 12 HAUL ROUTE & MECHANICAL OFFLOAD WORKPOINT
19	41°31'39.81"N	81°40'4.47"W	+590'	20'	+610'	+620'	NE CORNER CDF 12 STOCKPILE AREA
20	41°31'37.48"N	81°40'9.38"W	+589'	20'	+609'	+614'	NW CORNER CDF 12 STOCKPILE AREA
21	41°31'32.70"N	81°40'7.05"W	+590'	20'	+610'	+611'	SW CORNER CDF 12 STOCKPILE AREA
22	41°31'37.17"N	81°39'57.91"W	+588'	20'	+608'	+628'	SE CORNER CDF 12 STOCKPILE AREA
23	41°31'39.46"N	81°40'04.92"W	+590'	20'	+610'	+619'	BENEFICAL REUSE EQUIPMENT STORAGE AREA
24	41°31'33.21"N	81°40'8.26"W	-	-	-	-	CORNER POINT CDF 9
25	41°31'24.82"N	81°40'26.48"W	-	-	-	-	CORNER POINT CDF 9
26	41°31'31.94"N	81°40'33.18"W	-	-	-	-	CORNER POINT CDF 9
27	41°31'43.27"N	81°40'13.27"W	-	-	-	-	CORNER POINT CDF 9 & 12
28	41°31'53.95"N	81°39'54.35"W	-	-	-	-	CORNER POINT CDF 12
29	41°31'43.60"N	81°39'44.05"W	-	-	-	-	CORNER POINT CDF 12
30	41°31'31.57"N	81°40'5.33"W	-	-	-	-	CORNER POINT CDF 12
31	41°31'31.13"N	81°40'5.31"W	-	-	-	-	CORNER POINT CDF 12
32	41°31'30.88"N	81°40'6.67"W	-	-	-	-	CORNER POINT CDF 12